

From: Planning (Shared)
Sent: 31 Jul 2018 10:42:17 +1000
To: Planning (Shared)
Subject: FW: Northern Building Approvals - Proposed Open Carport Structure at Unit 2 14 Sutherland Street Mareeba
Attachments: Mulungu Open Carport Plans.pdf, Mulungu Site Plan.pdf, Planning Report for Unit 2 14 Sutherland Street Mareeba.docx, DAForm1-DA 18 0001.docx

From: Northern Building Approvals <kentonstella@bigpond.com>
Sent: Monday, 30 July 2018 1:24 PM
To: Planning (Shared) <planning@msc.qld.gov.au>
Subject: Northern Building Approvals - Proposed Open Carport Structure at Unit 2 14 Sutherland Street Mareeba

Hi MSC Planning Team

Please see the attached document for the Proposed Open Carport Structure at Unit 2 14 Sutherland Street Mareeba:

1. DA Form 1
2. Report
3. Open Carport Structure site plan and plans

Please contact me for payment.

Kind Regards

Kenton Byrne
Northern Building Approvals
BBldSurvCert(Hons), MAIBS
Building Surveyor (LTD)
QBCC Certifiers Licence: A1107628
Mobile: 0447 865 265



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CLADDING

ITEM	PROFILE (min)	FINISH	COLOUR
ROOF	TRIMDEK 0.42 BMT	CB	SM
WALLS	-		
CORNERS	-		
BARGE	-	CB	CN
GUTTER	EMLINE	CB	CN

0.35bmt=0.40tct; 0.42bmt=0.47tct; 0.48bmt=0.53tct

ACCESSORY SCHEDULE & LEGEND

QTY	MARK	DESCRIPTION

ARCHITECTURAL DRAWING ONLY, NOT FOR CONSTRUCTION USE

WIND DESIGN

IMPORTANCE LEVEL	REGION	TERRAIN	Ms
2	C	2.5	1.0

CLIENT
Mulungu Health

SITE
**14 Sutherland Street
MAREEBA QLD 4880**

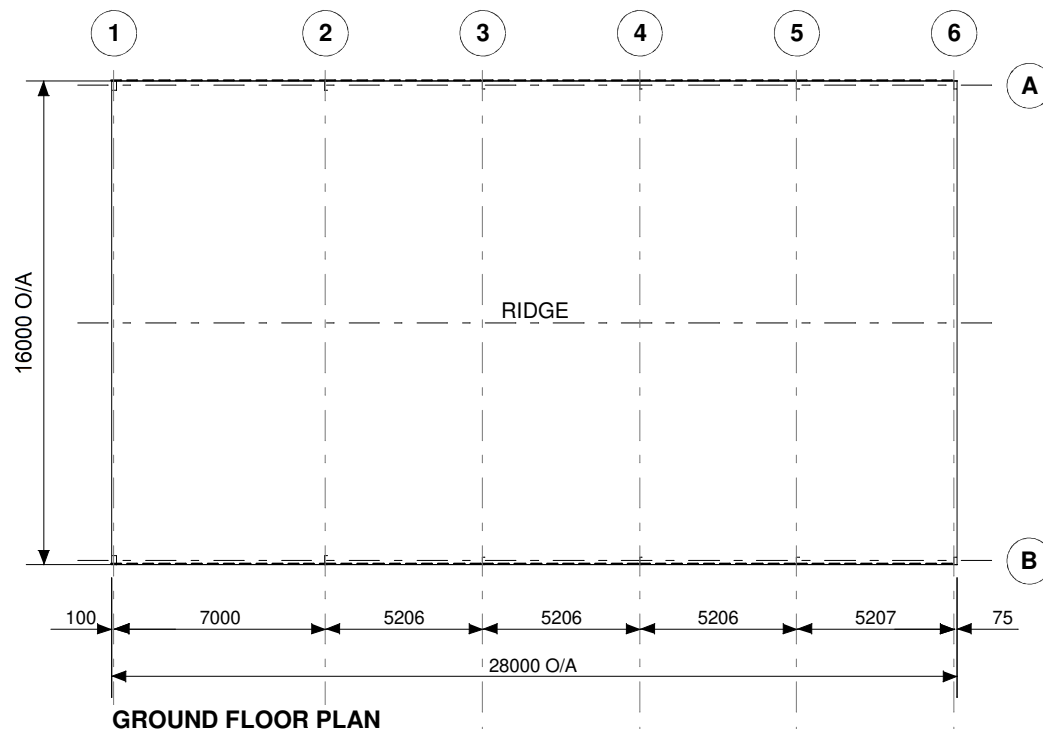
BUILDING
**COVERMASTER (CEE)
16000 SPAN x 4000 EAVE x 28000 LONG**

TITLE
GENERAL ARRANGEMENT

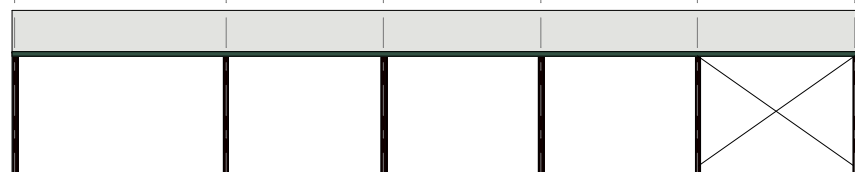
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DRAWING NUMBER
MAREB1-13118

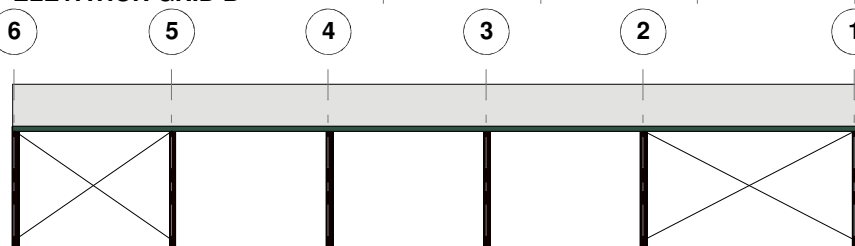
PAGE
1/2



GROUND FLOOR PLAN

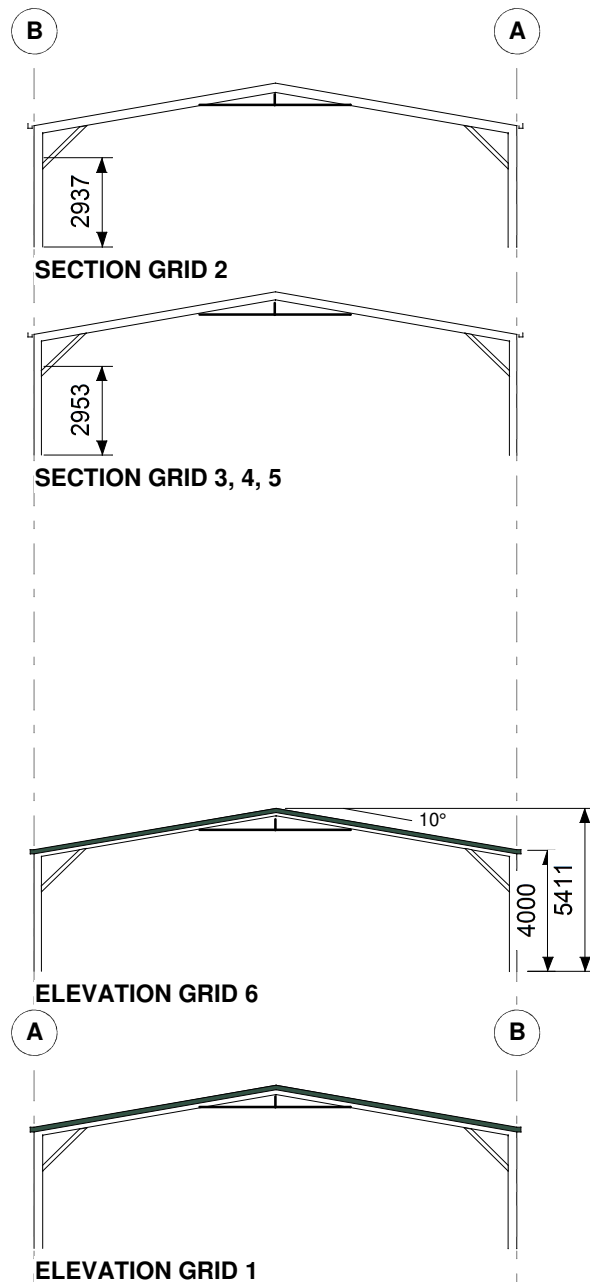


ELEVATION GRID B



ELEVATION GRID A

Cont. on page 2



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A4 SHEET 1:250

DRAWING NUMBER MAREB1-13118	PAGE 2/2
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FOR PAD FOOTING DIMENSIONS, REFER TO THE INTEGRAL PAD FOOTING SCHEDULE LOCATED ON A SEPARATE PAGE OF THIS DRAWING.

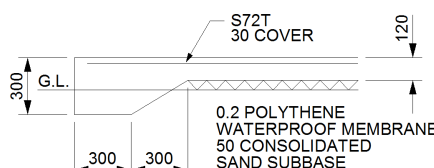
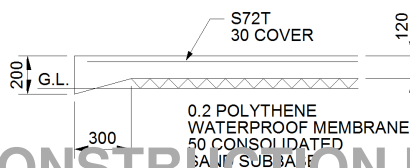
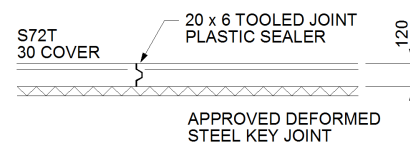
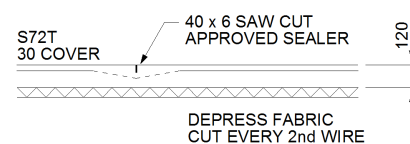
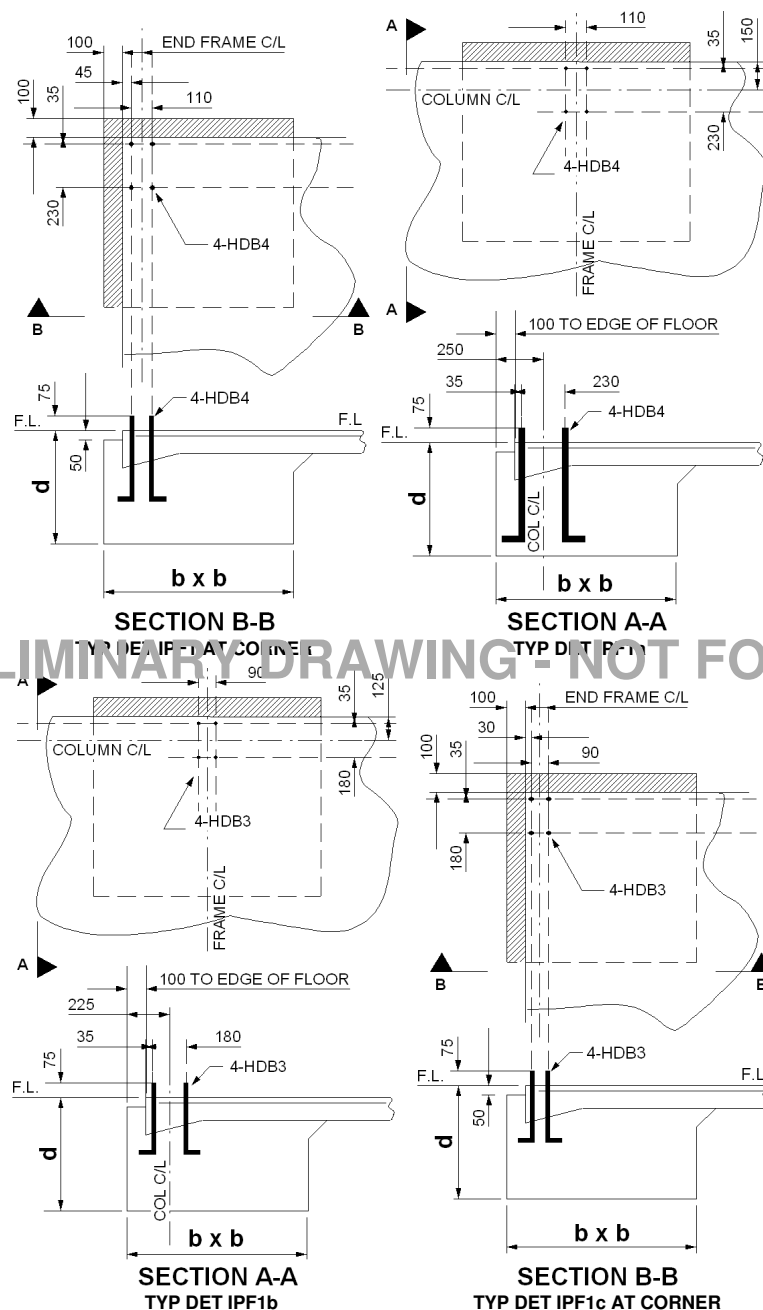
ALL DIMENSIONS SHOULD BE CHECKED AND VERIFIED PRIOR TO COMMENCEMENT OF ANY WORKS.

SEE ENGINEERING DRAWINGS FOR SITE NOTES CONCRETE REINFORCEMENT NOTES, CONCRETE NOTES, SLAB ON GRADE NOTES AND ADDITIONAL INTEGRAL PAD FOOTING NOTES.

PROVIDE CONSTRUCTION JOINTS SO THAT THE MAXIMUM UNBROKEN RUN OF CONCRETE IS 20m IN EITHER DIRECTION

IF SLIDING DOORS ARE INCLUDED ON THIS PROJECT, AN RC BEAM FOOTING IS NECESSARY TO SUPPORT THE BOTTOM TRACK SYSTEM, AND MUST BE POURED WITH THE INTEGRAL PAD FOOTINGS. REFER TO THIS DRAWING AND THE ASSEMBLY GUIDE FOR DETAILS PRIOR TO WORKS COMMENCEMENT. FOR SLIDING DOOR TRACK LOCATIONS, REFER TO MAREB1-13118

Cont. on page 2



CLIENT Mulungu Health		
SITE 14 Sutherland Street MAREEBA QLD 4880		
BUILDING COVERMASTER (CEE) 16000 SPAN x 4000 EAVE x 28000 LONG		
TITLE INTEGRAL PAD FOOTING & RC FLOOR DET		
SCALE NTS	DRAWING NUMBER IPF-13118	PAGE 1/4



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INTEGRAL PAD FOOTING SCHEDULE

CENTRE LINE REFERENCE	FRAME REFERENCE(S)	LABEL	d x b x b
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A	2	IPF1a	900 x 900 x 900
A	3, 4, 5	IPF1b	900 x 900 x 900
A	6	IPF1c	900 x 900 x 900
B	1	IPF1	900 x 900 x 900
B	2	IPF1a	900 x 900 x 900
B	3, 4, 5	IPF1b	900 x 900 x 900
B	6	IPF1c	900 x 900 x 900

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SCALE
NTS

DRAWING NUMBER
IPF-13118

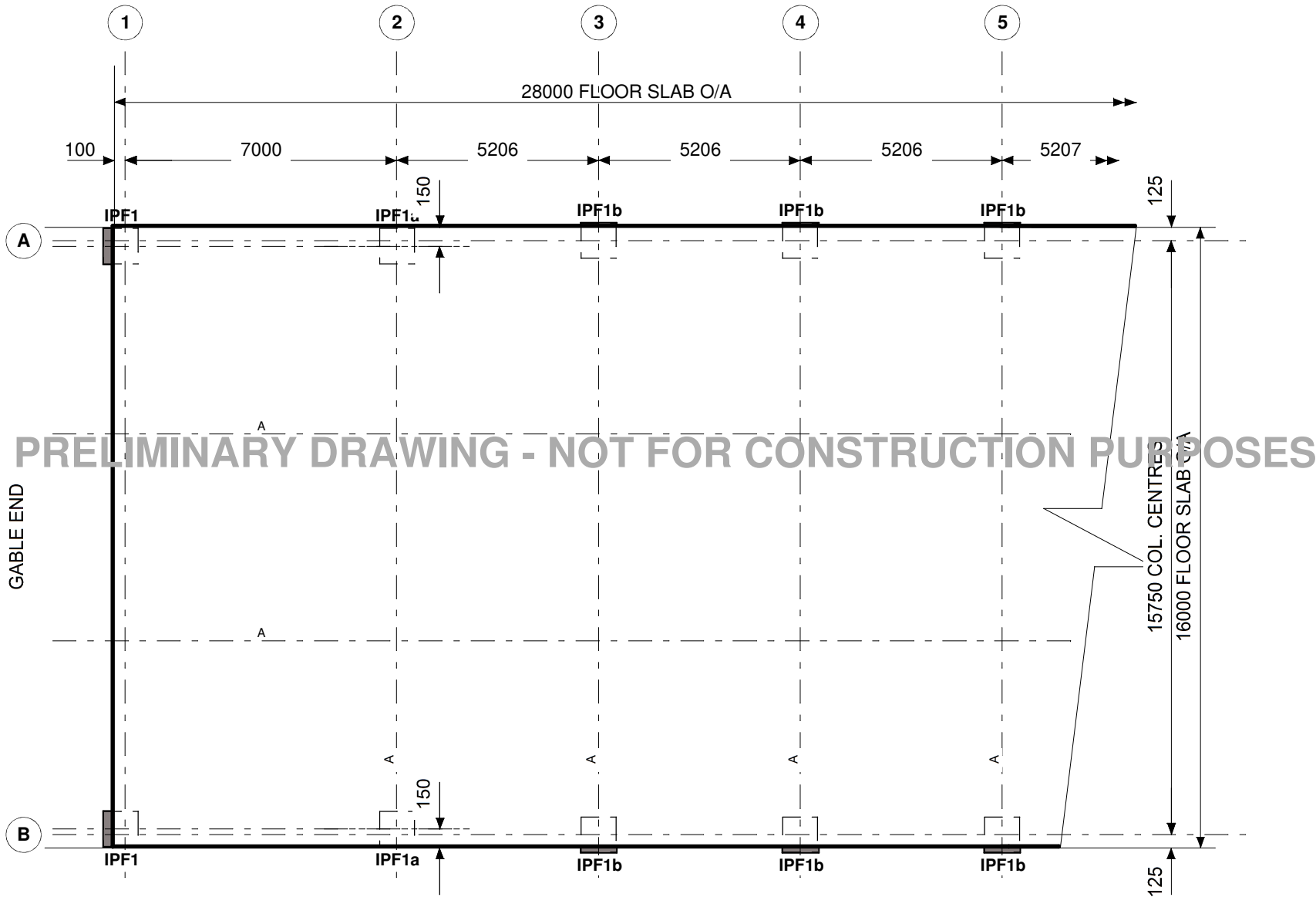
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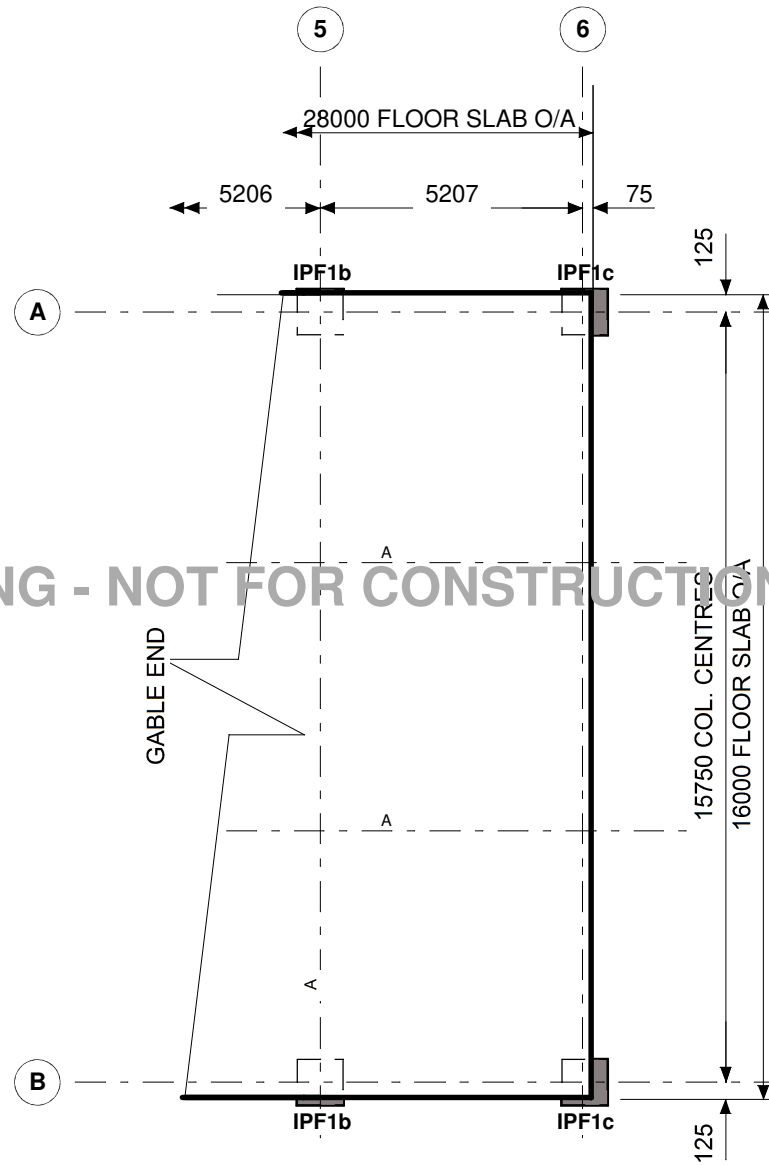
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PRELIMINARY DRAWING - NOT FOR CONSTRUCTION PURPOSES

SCALE
NTS

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IPF-13118

PAGE
4/4

FOR BORED PIER DIMENSIONS & REINFORCEMENT NOTATION, REFER TO THE BORED PIER SCHEDULE LOCATED ON A SEPARATE PAGE OF THIS DRAWING.

ALL DIMENSIONS SHOULD BE CHECKED AND VERIFIED PRIOR TO COMMENCEMENT OF ANY WORKS.

SEE ENGINEERING DRAWINGS FOR SITE NOTES CONCRETE REINFORCEMENT NOTES, CONCRETE NOTES, SLAB ON GRADE NOTES AND ADDITIONAL BORED PIER NOTES.

SEE DRG BPP-13118 FOR BORING PLAN.

PROVIDE CONSTRUCTION JOINTS SO THAT THE MAXIMUM UNBROKEN RUN OF CONCRETE IS 20m IN EITHER DIRECTION

IF SLIDING DOORS ARE INCLUDED ON THIS PROJECT, AN RC BEAM FOOTING IS NECESSARY TO SUPPORT THE BOTTOM TRACK SYSTEM, AND MUST BE POURED WITH THE BORED PIERS. REFER TO THIS DRAWING AND THE ASSEMBLY GUIDE FOR DETAILS PRIOR TO WORKS COMMENCEMENT. FOR SLIDING DOOR TRACK LOCATIONS, REFER TO MAREB1-13118

Cont. on page 2

CLIENT
Mulungu Health

SITE
**14 Sutherland Street
MAREEBA QLD 4880**

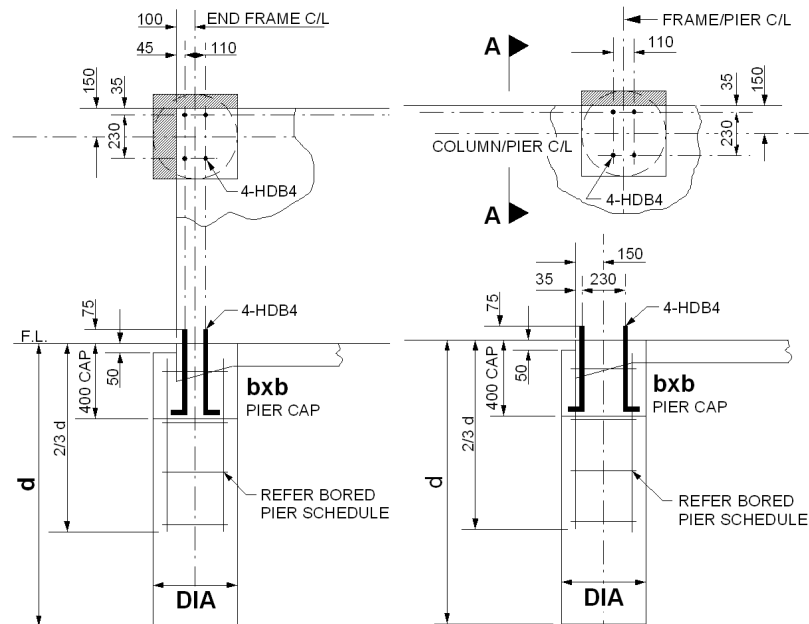
BUILDING
**COVERMASTER (CEE)
16000 SPAN x 4000 EAVE x 28000 LONG**

TITLE
BORED PIER & RC SLAB

SCALE
NTS

DRAWING NUMBER
BPS-13118

PAGE
1/5

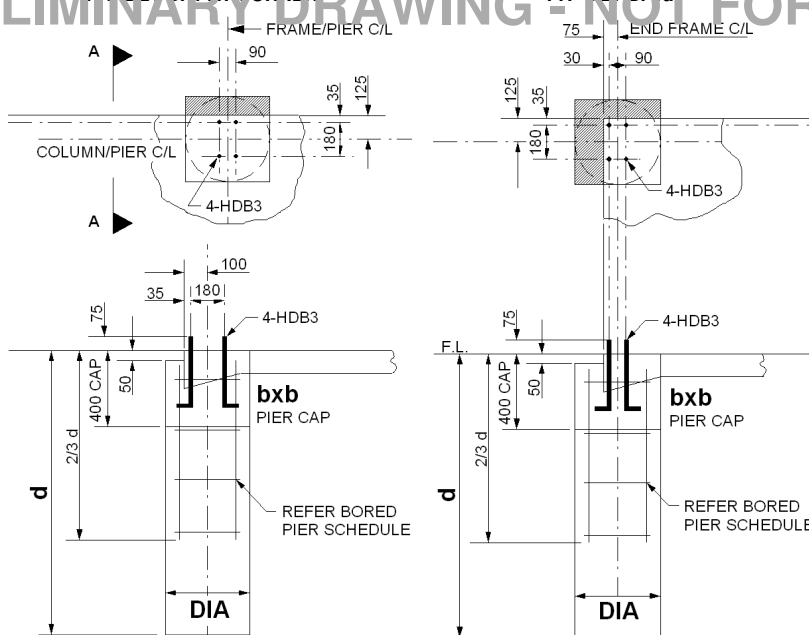


SECTION B - B

TYP DET BP1a CORNER

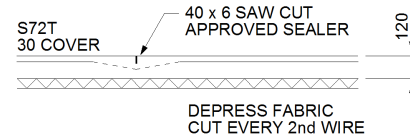
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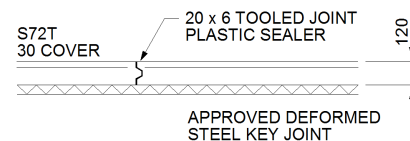


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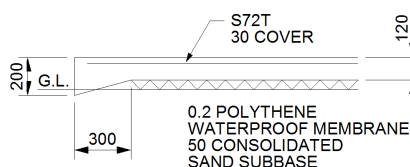
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TYP DET BP1c AT CORNER



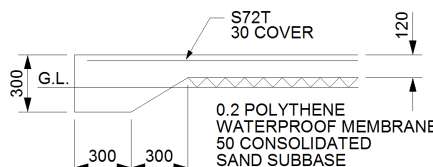
DET S1/A
CONTROL JOINT



DET S1/C
CONSTRUCTION JOINT



DET S1/EB1
NOT SUITABLE AT OPENINGS
SUBJECT TO VEHICLE TRAFFIC



DET S1/EB2
REQUIRED AT OPENINGS
SUBJECT TO VEHICLE TRAFFIC

WHERE WALLS ARE SHEETED SET DOWN
SHADED AREAS 50mm

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PRELIMINARY DRAWING - NOT FOR CONSTRUCTION PURPOSES



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BORED PIER SCHEDULE FOR PIER CAPS CAST INTEGRAL WITH RC FLOOR SLAB

CENTRE LINE REFERENCE	FRAME REFERENCE(S)	LABEL	DIA x d	PIER CAP (b x b)	REINFORCEMENT
A	1	BP1	600 x 1650	600 x 600	6N16 VERT R6 TIES @ 300
A	2	BP1a	600 x 1650	600 x 600	6N16 VERT R6 TIES @ 300
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A	6	BP1c	600 x 1650	600 x 600	6N16 VERT R6 TIES @ 300
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B	2	BP1a	600 x 1650	600 x 600	6N16 VERT R6 TIES @ 300
B	3, 4, 5	BP1b	600 x 1650	600 x 600	6N16 VERT R6 TIES @ 300
B	6	BP1c	600 x 1650	600 x 600	6N16 VERT R6 TIES @ 300

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BORED PIER SCHEDULE FOR PIERS WITH NO FLOOR OR ISOLATED FROM RC FLOOR SLAB

CENTRE LINE REFERENCE	FRAME REFERENCE(S)	LABEL	DIA x d	PIER CAP (b x b)	REINFORCEMENT
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B	1	BP1	600 x 2850	600 x 600	6N16 VERT R6 TIES @ 300
B	2	BP1a	600 x 2850	600 x 600	6N16 VERT R6 TIES @ 300
B	3, 4, 5	BP1b	600 x 2850	600 x 600	6N16 VERT R6 TIES @ 300
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SCALE
NTS

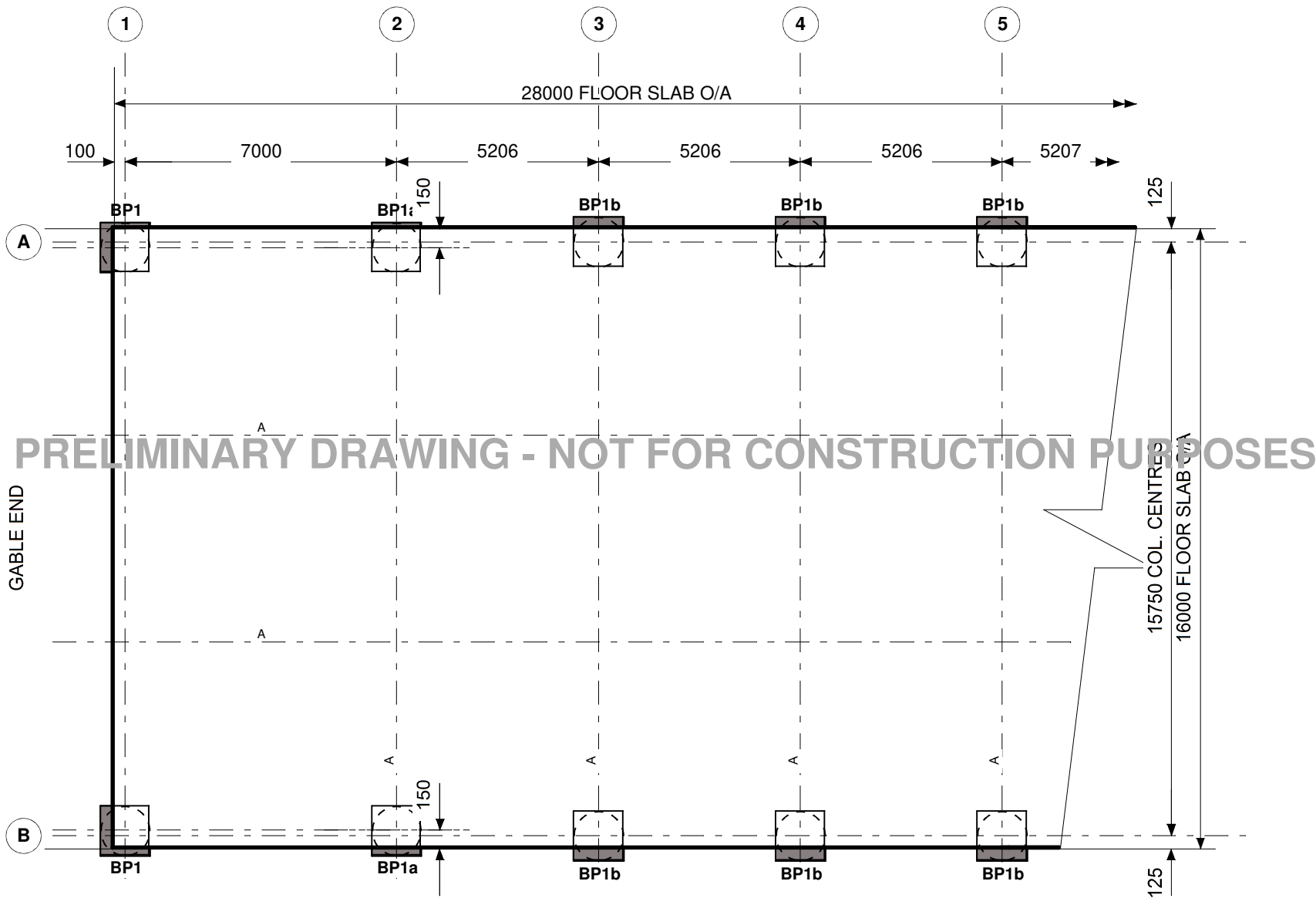
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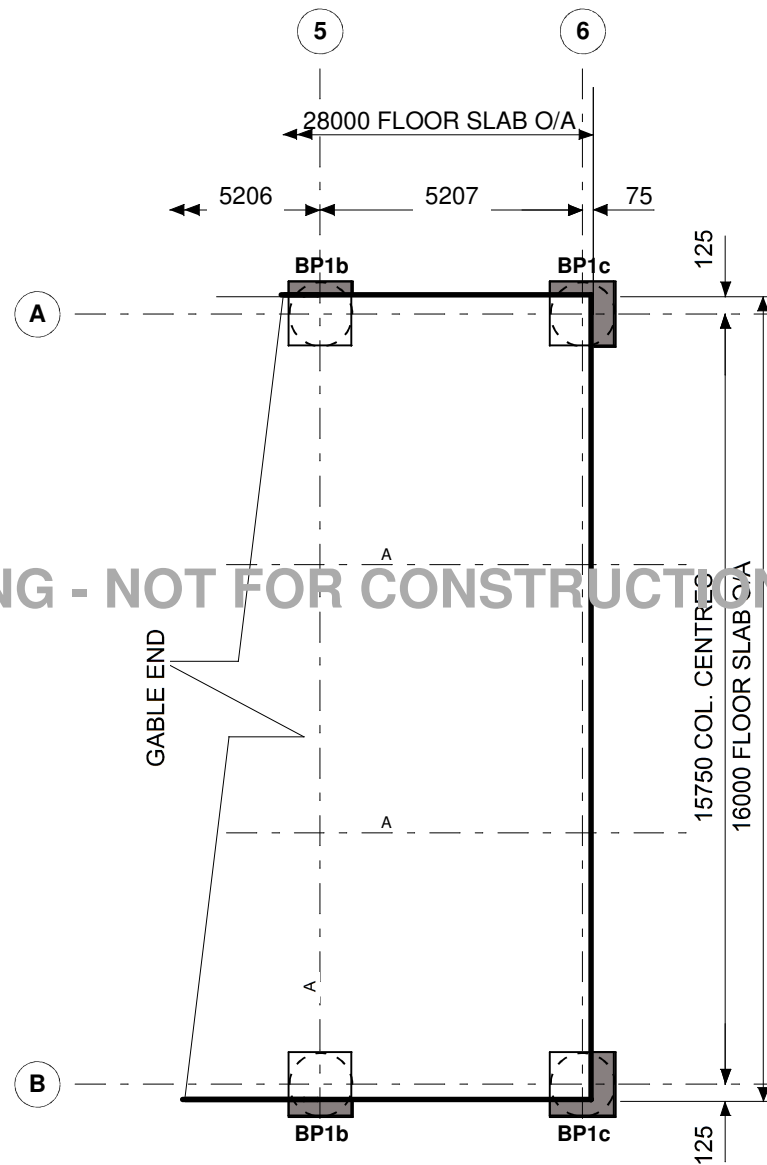
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Mulungu Health



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 **Queensland Globe**



5 metres

Paper Size: A4
Print Date: 19/2/2018

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Legend

Local government




Property



Address

Railway Stop

 Railway Station


 Railway Siding Point

 Cane Railway Siding Point

Road crossing


 Bridge

 Overpass

 Tunnel

Land parcel label


Land parcel


 Parcel

Railway Line

 Government, Electrified

 Government, Non Electrified

 Private, Electrified

 Private, Non Electrified

Road

 Highway

 Main

 Local


 Private

 Track

 Mall

 Ferry

 Path

 Busway

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Planning Report for Unit 2 14 Sutherland Street Mareeba

Material Change of Use for Proposed Open Carport Structure to Existing Community Care Centre Development

Prepared for: Mulungu Aboriginal Corporation

Site Description

Unit 2 14 Sutherland Street is located on the eastern side of Mareeba. The property is described as 4/RP717124. The subject lot is 1442m² in size. The land is currently zoned Medium Density Residential under the current Mareeba Shire Planning Scheme. The exist buildings on the property include office building and two carports which be removed for the proposed larger open carport structure. Below is an image showing the subject land.



Development Proposal

This application is for Building Works assessable against the Mareeba Shire Planning Scheme:

- Level of Assessment — Impact Assessment

- Assessment Criteria:
 - a. Medium density residential code
 - b. Community activities code
 - c. Landscaping code
 - d. Parking and access code
 - e. Works, services and infrastructure

Attachment 1 is the Proposed Carport Structure Plans.

Referrals

No referrals are required.

Planning Assessment Summary

This development is for proposed Open Carport Structure that will provide cover over the existing vehicle accommodation onsite. The proposed development is within the Medium Density Residential Zone of the Mareeba as identified on the Zone Map ZM016b and close (walking distance) of the Centre zone. The proposed development requires an impact assessment only because it is not a defined use in the Medium Density Residential Zone.

The proposed development can comply with the acceptable outcomes of the applicable assessment codes, and the proposed design illustrates an appropriate balance with performance outcomes. Additionally, the proposed development does not cause amenity impacts beyond the reasonable expectation for the Medium Density Residential Zone as the proposed open structure has 2.0m setback with proposed landscaping and 1.8 high solid screen fence to the side and rear boundary.

Therefore, the development is consistent with applicable assessment codes. The fact is that the development is appropriate for this lot and generally complies with all relevant aspects of the planning scheme. Your swift action to approve this development is appreciated.

Mandatory Supporting Information

Assessment of application against relevant Development Codes

The following Development Codes are considered to be applicable to the assessment of the application:

- 6.2.7 Medium density residential zone code
- 9.3.3 Community activities code
- 9.4.2 Landscaping code
- 9.4.3 Parking and access code
- 9.4.5 Works, services and infrastructure code

6.2.7 Medium density residential zone code

6.2.7.1 Application

- (1) This code applies to assessing development where:
 - (a) located in the Medium density residential zone; and
 - (b) it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

6.2.7.2 Purpose

- (1) The purpose of the medium density residential zone code is to provide for medium density multiple dwellings supported by community uses and small-scale services and facilities that cater for local residents.
- (2) Mareeba Shire Council's purpose of the Medium density residential zone code is to facilitate medium residential densities and a diversity of housing which caters for a range of households in locations which are proximate to town centres, community facilities and open space.

Small lot housing is facilitated and medium density development may include Dual occupancy and Multiple dwelling development in the form of town houses, apartments and units.

- (3) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Development provides a range of residential dwelling choices including Multiple dwellings in locations clustered around or near activity centres and transport networks;
 - (b) Development encourages and facilitates urban consolidation and the efficient use of physical and social infrastructure;
 - (c) Development is supported by employment nodes, community facilities and services, transport and commercial hubs where appropriate; Development provides and maintains a high level of amenity in the zone and is reflective of the desired character of the area;
 - (d) The scale and density of development facilitates an efficient land use pattern that supports safe and walkable neighbourhoods that are well connected to employment nodes, centres, open space and recreational areas, community services and educational opportunities;
 - (e) Other small-scale development that integrates personal employment and residential activities is encouraged, provided it complements local residential amenity;
 - (f) Development maintains a high level of residential amenity avoiding uses that introduce impacts associated with noise, hours of operation, traffic, advertising devices, visual amenity, privacy, lighting, odour and emissions;
 - (g) Non-residential development may be supported where such uses directly support the day to day needs of the immediate residential community; and
 - (h) Development responds to land constraints and mitigates any adverse impacts on adjacent land uses and the environment.

6.2.7.3 Criteria for assessment

Table 6.2.7.3A—Medium density residential zone code - For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
Height			
PO1 Building height takes into consideration and respects the following: <ul style="list-style-type: none"> (a) the height of existing buildings on adjoining premises; (b) the development potential, with respect to height, on adjoining premises; (c) the height of buildings in the vicinity of the site; (d) access to sunlight and daylight for the site and adjoining sites; (e) privacy and overlooking; and (f) site area and street frontage length. 	AO1 Development has a maximum building height of: <ul style="list-style-type: none"> (a) 8.5 metres; and (b) 2 storeys above ground level. 	✓	The proposed Open Carport Structure development is <8.5m high and single storey.
Outbuildings and residential scale			
PO2 Domestic outbuildings: <ul style="list-style-type: none"> (a) do not dominate the lot on which they are located; and (b) are consistent with the scale and character of development in the Medium density residential zone. 	AO2 Domestic outbuildings do not exceed: <ul style="list-style-type: none"> (a) 100m² in gross floor area; and (b) 5.5 metres in height above natural ground level. 	N / A	No domestic outbuilding proposed in this application.
Siting			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO3 Development is sited in a manner that considers and respects: <ul style="list-style-type: none"> (a) the siting and use of adjoining premises; (b) access to sunlight and daylight for the site and adjoining sites; (c) privacy and overlooking; (d) opportunities for casual surveillance of adjoining public spaces; (e) air circulation and access to natural breezes; (f) appearance of building bulk; and (g) relationship with road corridors. 	AO3.1 Buildings and structures include a minimum setback of: <ul style="list-style-type: none"> (a) 6 metres from the primary road frontage; and (b) 3 metres from any secondary road frontage. 	✓	The proposed Open Carport Structure development has a compliant front road boundary setback as it is proposed to the rear of the property.
	AO3.2 Buildings and structures include a minimum setback of 2 metres from side and rear boundaries.	✓	The proposed Open Carport Structure development has a side and rear setback of 2.0m. Therefore, the structure will not affect the privacy and outlook of these properties.
Accommodation density			
PO4 The density of Accommodation activities: <ul style="list-style-type: none"> (a) contributes to housing choice and affordability; (b) respects the nature and density of surrounding land use; (c) does not cause amenity impacts beyond the reasonable expectation of accommodation density for the zone; and 	AO4 Development provides a maximum density for Accommodation activities in compliance with Table 6.2.7.3B .	N/A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
(d) is commensurate to the scale and frontage of the site.			
Gross floor area			
PO5 Buildings and structures occupy the site in a manner that: <ul style="list-style-type: none"> (a) makes efficient use of land; (b) is consistent with the bulk and scale of surrounding buildings; and (c) appropriately balances built and natural features. 	AO5 Gross floor area does not exceed 600m ² .	✓	The proposed Open Carport Structure development has a gross floor area of 448m ² . Additionally, the location will maintain the natural features of property (landscaping to the front road boundary and northern side boundary) and the efficient use of the land by using the existing driveway to the location.
For assessable development			
Building design			
PO6 Building facades are appropriately designed to: <ul style="list-style-type: none"> (a) include visual interest and architectural variation; (b) maintain and enhance the character of the surrounds; (c) provide opportunities for casual surveillance; 	AO6 Buildings include habitable space, pedestrian entrances and recreation space facing the primary road frontage.	✓	The proposed Open Carport Structure is intended to be located at the rear of the property and will not change the existing building façade.

Performance outcomes	Acceptable outcomes	Complies	Comments
(d) include a human scale; and (e) encourage occupation of outdoor space.			
PO7 Development complements and integrates with the established built character of the Medium density residential zone, having regard to: (a) roof form and pitch; (b) eaves and awnings; (c) building materials, colours and textures; and (d) window and door size and location.	AO7 No acceptable outcome is provided.	✓	The proposed Open Carport Structure is intended to be located at the rear of the property and will have limited effect on the established built character of the street scape.
Non-residential development			
PO8 Non-residential development: (a) is consistent with the scale of existing development; (b) does not detract from the amenity of nearby residential uses; (c) directly supports the day to day needs of the immediate residential community; and (d) does not impact on the orderly provision of non-residential development in other locations in the shire.	AO8 No acceptable outcome is provided.	✓	The proposed Open Carport Structure is intended to be located at the rear of the property and will have limited effect on the established built character of the street.
Amenity			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO9 Development must not detract from the amenity of the local area, having regard to: (a) noise; (b) hours of operation; (c) traffic; (d) advertising devices; (e) visual amenity; (f) privacy; (g) lighting; (h) odour; and (i) emissions.	AO9 No acceptable outcome is provided.	✓	The proposed Carport Structure will provide cover over the existing vehicle accommodation onsite which is mainly use during business hours. The proposed 2.0m setback to the open structure and additional landscaping and to 1.8m high solid screen fence to the rear boundary will limit any visual amenity impact.
PO10 Development must take into account and seek to ameliorate any existing negative environmental impacts, having regard to: (a) noise; (b) hours of operation; (c) traffic; (d) advertising devices; (e) visual amenity; (f) privacy; (g) lighting; (h) odour; and (i) emissions.	AO10 No acceptable outcome is provided.	✓	The proposed Carport Structure will provide cover over the existing vehicle accommodation onsite which is mainly use during business hours. The proposed 2.0m setback to the open structure and additional landscaping and to 1.8m high solid screen fence to the rear boundary will limit any visual amenity impact.

Table 6.2.7.3B—Maximum densities for Accommodation activities

Use	Maximum density
Dual occupancy	1 dwelling per 300m ² of site area
Multiple dwelling	(a) 1 dwelling per 150m ² of site area; and (b) 1 bedroom per 75m ² of site area.
Residential care facility	1 dwelling or accommodation unit per 100m ² of site area.
Retirement facility	1 dwelling or accommodation unit per 150m ² of site area

9.3.3 Community activities code

9.3.3.1 Application

- (1) This code applies to assessing development where:
 - (a) involving Community activities; and
 - (b) it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

9.3.3.2 Purpose

- (1) The purpose of the Community activities code is to ensure Community activities that are appropriately designed and located to:
 - (a) be conveniently accessible to the communities they serve; and
 - (b) not detrimentally impact on local amenity.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Community activities do not have an adverse impact on the amenity of the surrounding area;
 - (b) Community activities establish in appropriate locations that are highly accessible;
 - (c) Community activities are located with other community activities when the opportunity to achieve practical co-location of facilities exists;
 - (d) Community activities are designed to be accessible, functional and safe;
 - (e) Community activities are compatible and integrated with surrounding land uses; and
 - (f) Community activities enhance community identity and character.

9.3.3.3 Criteria for assessment

Table 9.3.3.3—Community activities code - For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
Amenity and privacy			

Performance outcomes	Acceptable outcomes	Complies	Comments
<p>PO1</p> <p>Community activities are appropriately located and designed to avoid adverse impacts on sensitive uses related to:</p> <ul style="list-style-type: none"> (a) noise; (b) lighting; and (c) overlooking. <p>Note—These provisions apply to any adjoining sensitive use, both on an adjoining site and on the same site as the Community activity.</p>	<p>AO1</p> <p>Along any common boundary with a sensitive land use, development incorporates:</p> <ul style="list-style-type: none"> (a) a 1.8 metre high solid screen fence; and (b) screening to windows which: <ul style="list-style-type: none"> (i) face the boundary; (ii) have a sill height less than 1.5 metres; and (iii) are not wholly screened by the boundary fence. 	✓	<p>The proposed Carport Structure will provide cover over the existing vehicle accommodation onsite which is mainly use during business hours. The proposed 2.0m setback to the open structure and additional landscaping and to 1.8m high solid screen fence to the rear boundary will limit any visual amenity impact.</p>
If for Educational establishment or Child care centre			
<p>PO2</p> <p>Development is located on a site that is capable of accommodating:</p> <ul style="list-style-type: none"> (a) all facilities necessary for the use; (b) required landscaping and buffering; and (c) appropriately designed access, manoeuvring and parking areas. 	<p>AO2</p> <p>The development is located on a site with a minimum:</p> <ul style="list-style-type: none"> (a) site area of 800m²; (b) road frontage of 20 metres; and (c) road reserve width of 20 metres. 	N/A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
PO3 The design of the development does not result in any safety hazard for children or other users of the facility.	A03 A child proof fence or physical barrier is provided to prevent unintended access to the following areas, directly from indoor or outdoor areas intended to accommodate children: (a) Vehicle manoeuvring and parking areas; (b) Refuse storage and servicing areas; and (c) Air conditioning, refrigeration plant and mechanical plant.	N/A	N/A
For assessable development			
Location			
A04 Development is compatible with the amenity of the surrounding area, having regard to: (a) the location and type of vehicular access and parking; (b) hours of operation; (c) waste storage and collection; (d) advertising devices and signage; (e) visual amenity; (f) privacy; and (g) noise, odour and dust emissions.	A04 No acceptable outcome is provided.	✓	The proposed Carport Structure will provide cover over the existing vehicle accommodation onsite which is mainly use during business hours. The proposed 2.0m setback to the open structure and additional landscaping and to 1.8m high solid screen fence to the rear boundary will limit any visual amenity impact.
PO5 Community activities are highly accessible to the community they serve and are located to encourage	A05.1 Community activities are not located in a cul-de-sac.	N/A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
multi-purpose trips.	A05.2 Development is located: <ul style="list-style-type: none"> (a) within 800 metres walking distance of the Centre zone; or (b) within 400 metres walking distance of a public transport stop; or (c) provided with a connection to the pedestrian and cycle network. 	✓	The proposed development is within 400m walking distance of the Centre zone.
Amenity and privacy			
PO6 Community activities protect and enhance the character and amenity of the locality and streetscape through the appropriate location and screening of: <ul style="list-style-type: none"> (a) air conditioning; (b) refrigeration plant; (c) mechanical plant; and (d) refuse bin storage areas. 	A06 No acceptable outcome is provided.	✓	The proposed development is open structure and will not require any air conditioning and can store refuse bin under cover and screening from the streetscape.
If for Educational establishment or Child care centre			
PO7 Educational establishments and Child care centres: <ul style="list-style-type: none"> (a) do not detrimentally impact on the amenity or operations of surrounding land uses; and (b) have suitable separation distances and buffering from sensitive uses. 	A07 No acceptable outcome is provided.	N/A	N/A

9.4.2 Landscaping code

9.4.2.1 Application

This code applies where it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

9.4.2.2 Purpose

- (1) The purpose of the Landscaping code is to ensure all development is landscaped to a standard that:
 - (a) complements the scale and appearance of the development;
 - (b) protects and enhances the amenity and environmental values of the site;
 - (c) complements and enhances the streetscape and local landscape character; and
 - (d) ensures effective buffering of incompatible land uses to protect local amenity.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Landscaping is a functional part of development design and is commensurate with the intended use;
 - (b) Landscaping accommodates the retention of existing significant on site vegetation where appropriate and practical;
 - (c) Landscaping treatments complement the scale, appearance and function of the development;
 - (d) Landscaping contributes to an attractive streetscape;
 - (e) Landscaping enhances the amenity and character of the local area;
 - (f) Landscaping enhances natural environmental values of the site and the locality;
 - (g) Landscaping provides effective screening both on site, if required, and between incompatible land uses;
 - (h) Landscaping provides shade in appropriate circumstances;
 - (i) Landscape design enhances personal safety and reduces the potential for crime and vandalism; and
 - (j) Intensive land uses incorporate vegetated buffers to provide effective screening of buildings, structures and machinery associated with the use.

9.4.2.3 Criteria for assessment

Table 9.4.2.3A—Landscaping code - For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
PO1 Development, other than in the Rural zone, includes landscaping that: <ul style="list-style-type: none"> (a) contributes to the landscape character of the Shire; (b) compliments the character of the immediate surrounds; (c) provides an appropriate balance between built and natural elements; and (d) provides a source of visual interest. 	AO1 Development, other than in the Rural zone, provides: <ul style="list-style-type: none"> (a) a minimum of 10% of the site as landscaping; (b) planting in accordance with Planning Scheme Policy 6 - Landscaping and preferred plant species; (c) for the integration of retained significant vegetation into landscaping areas; (d) on-street landscaping works in accordance with the Design Guidelines set out in Section D9 Landscaping, of the Planning Scheme Policy 4 - FNQROC Regional Development Manual. <p>Note—Where development exceeds a site cover of 90%, areas of landscaping may be provided above ground level to achieve a total supply of landscaping equivalent to 10% of the site area.</p>	✓	The proposed development provides more than 10% of the site as landscaping. The site cover of the buildings is around 50%. Therefore, the building coverage is not high and the open space areas is great, this amount of landscaping provide is appropriate for the balance between built and natural elements is practical.

Performance outcomes	Acceptable outcomes	Complies	Comments
<p>PO2 Development, other than in the Rural zone, includes landscaping along site frontages that:</p> <ul style="list-style-type: none"> (a) creates an attractive streetscape; (b) compliments the character of the immediate surrounds; (c) assists to break up and soften elements of built form; (d) screen areas of limited visual interest or servicing; (e) provide shade for pedestrians; and (f) includes a range and variety of planting. 	<p>AO2 Development, other than in the Rural zone, includes a landscape strip along any site frontage:</p> <ul style="list-style-type: none"> (a) with a minimum width of 2 metres where adjoining a car parking area; (b) with a minimum width of 1.5 metres in all other locations; and (c) in accordance with Planning Scheme Policy 6 - Landscaping and preferred plant species. <p>Note—Where development is setback from a frontage less than 1.5 metres, the setback area is provided as a landscape strip</p>	✓	<p>The proposed development will maintain the natural features of property (landscaping to the front road boundary and northern side boundary) and will provide a additional landscaping and to 1.8m high solid screen fence where adjoining the rear boundary. Additionally, this proposed landscaping trip complies with the necessary landscaping required adjoining a car parking area and creates an attractive streetscape and soften elements of built form.</p>
<p>PO3 Development includes landscaping and fencing along side and rear boundaries that:</p> <ul style="list-style-type: none"> (a) screens and buffer land uses; (b) assists to break up and soften elements of built form; (c) screens areas of limited visual interest; (d) preserves the amenity of sensitive 	<p>AO3.1 Development provides landscape treatments along side and rear boundaries in accordance with Table 9.4.2.3B.</p>	✓	<p>1.8m high screen fencing provided along all side and rear boundaries.</p>

Performance outcomes	Acceptable outcomes	Complies	Comments
(e) land uses; and includes a range and variety of planting.	AO3.2 Shrubs and trees provided in landscape strips along side and rear boundaries: <ul style="list-style-type: none"> (a) are planted at a maximum spacing of 1 metre; (b) will grow to a height of at least 2 metres; (c) will grow to form a screen of no less than 2 metres in height; and (d) are mulched to a minimum depth of 0.1 metres with organic mulch. 	✓	Can be conditioned to comply.
	AO3.3 Any landscape strip provided along a side or rear boundary is designed in accordance with Planning Scheme Policy 6 - Landscaping and preferred plant species.	✓	Can be conditioned to comply.

Performance outcomes	Acceptable outcomes	Complies	Comments
PO4 Car parking areas are improved with a variety of landscaping that: <ul style="list-style-type: none"> (a) provides visual interest; (b) provides a source of shade for pedestrians; (c) assists to break up and soften elements; and (d) improves legibility. 	AO4.1 Landscaping is provided in car parking areas which provides: <ul style="list-style-type: none"> (a) a minimum of 1 shade tree for every 4 parking spaces, or part thereof, where the car parking area includes 12 or more spaces; (b) a minimum of 1 shade tree for every 6 parking spaces, or part thereof, otherwise; and (c) where involving a car parking area in excess of 500m²: <ul style="list-style-type: none"> (i) shade structures are provided for 50% of parking spaces; and (ii) a minimum of 10% of the parking area as landscaping. <p>Note—Where a shade structure is provided over part of a car parking area, shade tree planting is not required in this area of the car parking area.</p>	N / A	N/A
	AO4.2 Landscaping in car parking areas is designed in accordance with Planning Scheme Policy 6 - Landscaping and preferred plant species.	N / A	N/A
PO5 Landscaping areas include a range and variety of planting that: <ul style="list-style-type: none"> (a) is suitable for the intended purpose and local conditions; 	AO5.1 Plant species are selected from the Plant Schedule in Planning Scheme Policy 6 - Landscaping and preferred plant species.	N / A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
(b) contributes to the natural character of the Shire; (c) includes native species; (d) includes locally endemic species, where practical; and (e) does not include invasive plants or weeds.	AO5.2 A minimum of 25% of (new and existing) plants is provided as larger, advanced stock with a minimum plant height of 0.7 metres and mulched to a minimum depth of 0.1 metres with organic mulch.	✓	Can be conditioned to comply.
PO6 Landscaping does not impact on the ongoing provision of infrastructure and services to the Shire.	AO6.1 Tree planting is a minimum of (a) 2 metres from any underground water, sewer, gas, electricity or telecommunications infrastructure; and (b) 4 metres from any inspection chamber.	✓	Can be conditioned to comply.
	AO6.2 Vegetation below or within 4 metres of overhead electricity lines and power poles has a maximum height of 3.5 metres at maturity.	✓	Can be conditioned to comply.
	AO6.3 Vegetation adjoining an electricity substation boundary, at maturity, will have: (a) a height of less than 4 metres; and (b) no foliage within 3 metres of the substation boundary, unless the substation has a solid wall along any boundary.	N / A	N/A
For assessable development			

Performance outcomes	Acceptable outcomes	Complies	Comments
P07 Landscaping areas are designed to: <ul style="list-style-type: none"> (a) be easily maintained throughout the ongoing use of the site; (b) allow sufficient area and access to sunlight and water for plant growth; (c) not cause a nuisance to occupants of the site or members of the public; and (d) maintain or enhance the safety of pedestrians through the use of Crime Prevention Through Environmental Design principles. 	A07 No acceptable outcome is provided.	✓	Can be conditioned to comply.

Table 9.4.2.3B—Side and rear boundary landscape treatments

Location or use	Landscape Strip Minimum Width	Screen Fencing Minimum Height	Extent of treatment
Where involving a use other than a dwelling house on a site with a common boundary with land in the Low density residential zone, the Medium density residential zone or the Rural residential zone:	1.5 metres	1.8 metres	Along the common boundary.

Note—Where more than one landscape treatment is applicable to a development in the above table, the development is to provide a landscape treatment that satisfies all applicable minimum specifications.

9.4.3 Parking and access code

9.4.3.1 Application

This code applies to assessing development where it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

9.4.3.2 Purpose

- (1) The purpose of the Parking and access code is to ensure:
 - (a) parking areas are appropriately designed, constructed and maintained;
 - (b) the efficient functioning of the development and the local road network; and
 - (c) all development provides sufficient parking, loading/service and manoeuvring areas to meet the demand generated by the use.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Land uses have a sufficient number of parking and bicycle spaces designed in a manner to meet the requirements of the user;
 - (b) Parking spaces and associated manoeuvring areas are safe, functional and provide equitable access;
 - (c) Suitable access for all types of vehicles likely to utilise a parking area is provided in a way that does not compromise the safety and efficiency of the surrounding road network;
 - (d) Premises are adequately serviced to meet the reasonable requirements of the development; and
 - (e) End of trip facilities are provided by new major developments to facilitate alternative travel modes.

9.4.3.3 Criteria for assessment

Table 9.4.3.3A—Parking and access code – For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
Car parking spaces			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO1 Development provides sufficient car parking to accommodate the demand likely to be generated by the use, having regard to the: <ul style="list-style-type: none"> (a) nature of the use; (b) location of the site; (c) proximity of the use to public transport services; (d) availability of active transport infrastructure; and (e) accessibility of the use to all members of the community. 	AO1 The number of car parking spaces provided for the use is in accordance with Table 9.4.3.3B . Note—Car parking spaces provided for persons with a disability are to be considered in determining compliance with AO1.	✓	The proposed development contains necessary carparks to the rear of the building and will exceed the required Minimum of 5 spaces per use or one space per 25m ² or part thereof of GFA. The proposed development permits vehicles to turn around and exit the site in a forward motion.
Vehicle crossovers			
PO2 Vehicle crossovers are provided to:: <ul style="list-style-type: none"> (a) ensure safe and efficient access between the road and premises; (b) minimize interference with the function and operation of roads; and (c) minimise pedestrian to vehicle conflict. 	AO2.1 Vehicular access to/from Council roads is designed and constructed in accordance with the Standard drawings in Planning Scheme Policy 4 - FNQROC Regional Development Manual.	✓	Can be conditioned to comply. The proposed development will have the ability to connect to road network via the front boundary with driveway/crossover in accordance with FNQROC Regional Development Manual

Performance outcomes	Acceptable outcomes	Complies	Comments
	AO2.2 Development on a site with two or more road frontages provides vehicular access from: <ul style="list-style-type: none"> (a) the primary frontage where involving Community activities or Sport and recreation activities, unless the primary road frontage is a State-controlled road; or (b) from the lowest order road in all other instances. 	N/A	N/A
	AO2.3 Vehicular access for particular uses is provided in accordance with Table 9.4.3.3E .	N / A	N/A
PO3 Access, manoeuvring and car parking areas include appropriate pavement treatments having regard to: <ul style="list-style-type: none"> (a) the intensity of anticipated vehicle movements; (b) the nature of the use that they service; and (c) the character of the surrounding locality. 	AO3 Access, manoeuvring and car parking areas include pavements that are constructed in accordance with Table 9.4.3.3C .	✓	Can be conditioned to comply. The proposed development will have the ability to include pavements that are constructed in accordance with Table 9.4.3.3C .
For assessable development			
Parking area location and design			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO4 Car parking areas are located and designed to: (a) ensure safety and efficiency in operation; and (b) be consistent with the character of the surrounding locality.	AO4.1 Car parking spaces, access and circulation areas have dimensions in accordance with AS/NZS 2890.1 Off-street car parking.	✓	Can be conditioned to comply. The proposed development will have the ability to be in accordance with AS/NZS 2890.1 Off-street car parking.
	AO4.2 Disabled access and car parking spaces are located and designed in accordance with AS/NZS 2890.6 Parking facilities - Off-street parking for people with disabilities.	N / A	N/A
	AO4.3 The car parking area includes designated pedestrian routes that provide connections to building entrances.	N / A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
	<p>AO4.4 Parking and any set down areas are:</p> <ul style="list-style-type: none"> (a) wholly contained within the site; (b) visible from the street where involving Commercial activities, Community activities, Industrial activities or a use in the Recreation and open space zone; (c) are set back behind the main building line where involving a Dual occupancy, Multiple dwelling, Residential care facility or Retirement facility; and (d) provided at the side or rear of a building in all other instances. 	✓	Parking and any visitor areas are wholly contained within the site and are set back behind the main building line. Additionally, the visitor's carpark will be visible from the street.
Site access and manoeuvring			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO5 Access to, and manoeuvring within, the site is designed and located to: <ul style="list-style-type: none"> (a) ensure the safety and efficiency of the external road network; (b) ensure the safety of pedestrians; (c) provide a functional and convenient layout; and (d) accommodate all vehicles intended to use the site. 	AO5.1 Access and manoeuvrability is in accordance with : <ul style="list-style-type: none"> (a) AS28901 – Car Parking Facilities (Off Street Parking); and (b) AS2890.2 – Parking Facilities (Off-street Parking) Commercial Vehicle Facilities. <p>Note—Proposal plans should include turning circles designed in accordance with AP34/95 (Austroads 1995) Design Vehicles and Turning Path Templates.</p>	✓	Can be conditioned to comply. The proposed development will have the ability to be in accordance with AS/NZS 2890.1 Off-street car parking.
	AO5.2 Vehicular access has a minimum sight distance in accordance with Part 5 of AUSTROADS.	✓	Can be conditioned to comply. The proposed development will have the ability to be in accordance with Part 5 of AUSTROADS.
	AO5.3 Vehicular access is located and designed so that all vehicles enter and exit the site in a forward gear.	✓	Can be conditioned to comply. The proposed development will have the ability to for all vehicles enter and exit the site in a forward gear.

Performance outcomes	Acceptable outcomes	Complies	Comments
	AO5.4 Pedestrian and cyclist access to the site: (a) is clearly defined; (b) easily identifiable; and (c) provides a connection between the site frontage and the entrance to buildings and end of trip facilities (where provided).	✓	The proposed development will have the ability to for pedestrian and cyclist to have the same access to the unit (and end of trip facilities/carports) by the paved driveway/crossover.
PO6 Development that involves an internal road network ensures that it's design: (a) ensure safety and efficiency in operation; (b) does not impact on the amenity of residential uses on the site and on adjoining sites, having regard to matters of: (i) hours of operation; (ii) noise (iii) light; and (iv) odour; (c) accommodates the nature and volume of vehicle movements anticipated to be generated by the use; (d) allows for convenient	AO6.1 Internal roads for a Tourist park have a minimum width of: (a) 4 metres if one way; or (b) 6 metres if two way.	N / A	N/A
	AO6.2 For a Tourist park, internal road design avoids the use of cul-de-sacs in favour of circulating roads, where unavoidable, cul-de-sacs provide a full turning circle for vehicles towing caravans having: (a) a minimum approach and departure curve radius of 12 metres; and (b) a minimum turning circle radius of 8 metres.	N / A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
<p>access to key on-site features by pedestrians, cyclists and motor vehicles; and</p> <p>(e) in the Rural zone, avoids environmental degradation.</p>	<p>AO6.3</p> <p>Internal roads are imperviously sealed and drained, apart from those for an Energy and infrastructure activity or Rural activity.</p>	N / A	N/A
	<p>AO6.4</p> <p>Speed control devices are installed along all internal roads, apart from those for an Energy and infrastructure activity or Rural activity, in accordance with Complete Streets.</p>	N / A	N/A
	<p>AO6.5</p> <p>Internal roads, apart from those for an Energy and infrastructure activity or Rural activity, are illuminated in accordance with AS 4282 (as amended) - Control of Obtrusive effects of outdoor lighting.</p>	N / A	N/A
	<p>AO6.6</p> <p>Where involving an accommodation activity, internal roads facilitate unobstructed access to every dwelling, accommodation unit, accommodation site and building by emergency services vehicles.</p>	N / A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
	AO6.7 For an Energy and infrastructure activity or Rural activity, internal road gradients: (a) are no steeper than 1:5; or (b) are steeper than 1:5 and are sealed.	N /A	N/A
Servicing			
PO7 Development provides access, maneuvering and servicing areas on site that: (a) accommodate a service vehicle commensurate with the likely demand generated by the use; (b) do not impact on the safety or efficiency of internal car parking or maneuvering areas; (c) do not adversely impact on the safety or efficiency of the road network; (d) provide for all servicing functions associated with the use; and (e) are located and designed to minimise their impacts on	AO7.1 All unloading, loading, service and waste disposal areas are located: (a) on the site; (b) to the side or rear of the building, behind the main building line; (c) not adjacent to a site boundary where the adjoining property is used for a sensitive use.	N /A	N/A
	AO7.2 Unloading, loading, service and waste disposal areas allow service vehicles to enter and exit the site in a forward gear.	N /A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
adjoining sensitive land uses and streetscape quality.	AO7.3 Development provides a servicing area, site access and maneuvering areas to accommodate the applicable minimum servicing vehicle specified in Table 9.4.3.3B .	N / A	N/A
Maintenance			
PO8 Parking areas are used and maintained for their intended purpose.	AO8.1 Parking areas are kept and used exclusively for parking and are maintained in a suitable condition for parking and circulation of vehicles.	✓	Can be conditioned to comply. The development can be conditioned to ensure that parking areas are kept and used exclusively for parking and are maintained in a suitable condition for parking and circulation of vehicles.
	AO8.2 All parking areas will be compacted, sealed, drained, line marked and maintained until such time as the development ceases.	✓	Can be conditioned to comply. The development can be conditioned to ensure that parking areas will be compacted, sealed, drained, line marked and maintained until such time as the development ceases.
End of trip facilities			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO9 Development within the Centre zone; Industry zone or Emerging community zone provides facilities for active transport users that: <ul style="list-style-type: none"> (a) meet the anticipated demand generated from the use; (b) comprise secure and convenient bicycle parking and storage; and (c) provide end of trip facilities for all active transport users. 	AO9.1 The number of bicycle parking spaces provided for the use is in accordance with Table 9.4.3.3D .	N / A	N/A
	AO9.2 End of trip facilities are provided in accordance with Table 9.4.3.3D .	N / A	N/A
If for Educational establishment or Child care centre where involving more than 100 vehicle movements per day or Renewable energy facility, Sport and recreation activities or Tourist park			
PO10 The level of traffic generated by the development on the surrounding local road network must not result in unacceptable impacts on adjacent land and local road users.	AO10 A traffic impact report is prepared by a suitably qualified person that identifies: <ul style="list-style-type: none"> (a) the expected traffic movements to be generated by the facility; (b) any associated impacts on the road network; and (c) any works that will be required to address the identified impacts. 	N / A	N/A
If for Educational establishment or Child care centre where involving more than 100 vehicle movements per day or Renewable energy facility, Sport and recreation activities or Tourist park			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO11 The level of traffic generated by the development on the surrounding local road network must not result in unacceptable impacts on adjacent land and local road users.	AO11 A traffic impact report is prepared by a suitably qualified person that identifies: <ul style="list-style-type: none"> (d) the expected traffic movements to be generated by the facility; (e) any associated impacts on the road network; and (f) any works that will be required to address the identified impacts. 	N / A	N/A

Table 9.4.3.3B—Vehicle Parking and Service Vehicle Space Requirements

Definition	Minimum number of Car parking spaces	Minimum Service Vehicle Space Provision
Community care centre	Minimum of 5 spaces per use or one space per 25m ² or part thereof of GFA, whichever is greater.	One SRV space.

Table 9.4.3.3C—Pavement Standards for Access, Manoeuvring and Car Parking areas

Zone	Compacted Gravel Base (minimum thickness)	Surfacing Options
All development other than dwelling house		
All zones other than the Conservation zone or the Rural zone	75mm	Reinforced concrete with a minimum thickness of: <ul style="list-style-type: none"> • 100mm for parking areas; and • 150mm for access ways.
	150mm	Asphalt with a minimum thickness of 25mm
	150mm	Two coat sprayed bitumen seal
	150mm	Concrete pavers
	Not applicable	Minimum 150mm thickness compacted gravel suitable for all weather and dust free

Note—Where more than one surfacing option is listed, any one of the treatments listed may be provided.

9.4.5 Works, services and infrastructure code

9.4.5.1 Application

- (1) This code applies to assessing development where it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

9.4.5.2 Purpose

- (1) The purpose of the Works, services and infrastructure code is to ensure that all development is appropriately serviced by physical infrastructure, public utilities and services and that work associated with development is carried out in a manner that does not adversely impact on the surrounding area.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Development provides an adequate, safe and reliable supply of potable, fire-fighting and general use water in accordance with relevant standards;
 - (b) Development provides for the treatment and disposal of wastewater and ensures there are no adverse impacts on water quality, public health, local amenity or ecological processes;
 - (c) Development provides for the disposal of stormwater and ensures that there are no adverse impacts on water quality or ecological processes;
 - (d) Development connects to the road network and any adjoining public transport, pedestrian and cycle networks while ensuring no adverse impacts on the safe, convenient and efficient operation of these networks;
 - (e) Development provides electricity and telecommunications services that meet its desired requirements;
 - (f) Development is connected to a nearby electricity network with adequate capacity without significant environment, social or amenity impact;
 - (g) Development does not affect the efficient functioning of public utility mains, services or installations;
 - (h) Infrastructure dedicated to Council is cost effective over its life cycle;
 - (i) Work associated with development does not cause adverse impacts on the surrounding area; and
 - (j) Development prevents the spread of weeds, seeds or other pests.

9.4.5.3 Criteria for assessment

Table 9.4.5.3 - Works, services and infrastructure code – For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
Water supply			

Performance outcomes	Acceptable outcomes	Complies	Comments
<p>PO1 Each lot has an adequate volume and supply of water that:</p> <ul style="list-style-type: none"> (a) meets the needs of users; (b) is adequate for fire-fighting purposes; (c) ensures the health, safety and convenience of the community; and (d) minimises adverse impacts on the receiving environment. 	<p>AO1.1 Development is connected to a reticulated water supply system in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual other than where located:</p> <ul style="list-style-type: none"> (a) in the Conservation zone, Rural zone or Rural residential zone; and (b) outside a reticulated water supply service area. 	<p>✓</p>	<p>Can be conditioned to comply.</p> <p>The existing development onsite is connected to Council's reticulated water supply system in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 - FNQROC Regional Development Manual.</p>

Performance outcomes	Acceptable outcomes	Complies	Comments
	<p>AO1.2 Development, where located outside a reticulated water supply service area and in the Conservation zone, Rural zone or Rural residential zone is provided with:</p> <ul style="list-style-type: none"> (a) a bore or bores are provided in accordance with the Design Guidelines set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual; or (b) on-site water storage tank/s: <ul style="list-style-type: none"> (i) with a minimum capacity of 90,000L; (ii) fitted with a 50mm ball valve with a camlock fitting; and (iii) which are installed and connected prior to the occupation or use of the development. 	N/A	N/A
Wastewater disposal			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO2 Each lot provides for the treatment and disposal of effluent and other waste water that: <ul style="list-style-type: none"> (a) meets the needs of users; (b) is adequate for fire-fighting purposes; (c) ensures the health, safety and convenience of the community; and (d) minimises adverse impacts on the receiving environment. 	AO2.1 Development is connected to a reticulated sewerage system in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual other than where located: <ul style="list-style-type: none"> (a) in the Conservation zone, Rural zone or Rural residential zone; and (b) outside a reticulated sewerage service area. 	✓	Can be conditioned to comply. The existing development onsite is connected to Council's reticulated sewerage supply system in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 - FNQROC Regional Development Manual.
	AO2.2 An effluent disposal system is provided in accordance with ASNZ 1547 On-Site Domestic Wastewater Management (as amended) where development is located: <ul style="list-style-type: none"> (a) in the Conservation zone, Rural zone or Rural residential zone; and (b) outside a reticulated sewerage service area. 	N/A	N/A
Stormwater infrastructure			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO3 Stormwater infrastructure is designed and constructed to collect and convey the design storm event to a lawful point of discharge in a manner that mitigates impacts on life and property.	AO3.1 Where located within a Priority infrastructure area or where stormwater infrastructure is available, development is connected to Council's stormwater network in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual.	✓	Can be conditioned to comply. The cross-fall of the property is about 0.3m and falls to the front road boundary. Therefore, development stormwater infrastructure can be designed and constructed to collect and convey the design storm event to a lawful point of discharge (to the kerb and channel) in accordance with FNQROC Regional Development Manual.

Performance outcomes	Acceptable outcomes	Complies	Comments
	<p>AO3.2 On-site drainage systems are constructed:</p> <p>(a) to convey stormwater from the premises to a lawful point of discharge; and</p> <p>(b) in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual.</p>	✓	<p>Can be conditioned to comply.</p> <p>The cross-fall of the property is about 0.3m and falls to the front road boundary. Therefore, development stormwater infrastructure can be designed and constructed to collect and convey the design storm event to a lawful point of discharge (to the kerb and channel) in accordance with FNQROC Regional Development Manual.</p>
Electricity supply			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO4 Each lot is provided with an adequate supply of electricity	AO4 The premises: (a) is connected to the electricity supply network; or (b) has arranged a connection to the transmission grid; or (c) where not connected to the network, an independent energy system with sufficient capacity to service the development (at near average energy demands associated with the use) may be provided as an alternative to reticulated electricity where: (i) it is approved by the relevant regulatory authority; and (ii) it can be demonstrated that no air or noise emissions; and (iii) it can be demonstrated that no adverse impact on visual amenity will occur.	✓	Can be conditioned to comply. The existing development onsite is connected to electricity supply network.
Telecommunications infrastructure			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO5 Each lot is provided with an adequate supply of telecommunication infrastructure	AO5 Development is provided with a connection to the national broadband network or telecommunication services.	✓	Can be conditioned to comply. The existing development onsite is connected to telecommunication network.
Existing public utility services			
PO6 Development and associated works do not affect the efficient functioning of public utility mains, services or installations.	AO6 Public utility mains, services are relocated, altered or repaired in association with the works so that they continue to function and satisfy the relevant Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual.	✓	This development encourages and facilitates urban consolidation and the efficient use of physical and social infrastructure within the Medium Density Residential Zone.
Excavation or filling			
PO7 Excavation or filling must not have an adverse impact on the: (a) streetscape; (b) scenic amenity; (c) environmental values; (d) slope stability; (e) accessibility; or (f) privacy of adjoining premises.	AO7.1 Excavation or filling does not occur within 1.5 metres of any site boundary.	✓	The cross-fall of the property is about 0.3m and falls to the front road boundary. Therefore, any future excavation and/or filling will be minimal.
	AO7.2 Excavation or filling at any point on a lot is to be no greater than 1.5 metres above or below natural ground level.	N/A	The cross-fall of the property is about 0.3m and therefore, any future excavation and/or filling will be minimal and not greater than 1.5 metres above or below natural ground level.

Performance outcomes	Acceptable outcomes	Complies	Comments
	A07.3 Earthworks batters: (a) are no greater than 1.5 metres in height; (b) are stepped with a minimum width 2 metre berm; (c) do not exceed a maximum of two batters and two berms (not greater than 3.6 metres in total height) on any one lot; (d) have a slope no greater than 1 in 4; and (e) are retained.	✓	The cross-fall of the property is about 0.3m and falls to the front road boundary. Therefore, any future excavation and/or filling will be minimal.
	A07.4 Soil used for filling or spoil from excavation is not stockpiled in locations that can be viewed from: (a) adjoining premises; or (b) a road frontage, for a period exceeding 1 month from the commencement of the filling or excavation.	✓	Can be conditioned to comply.
	A07.5 All batters and berms to be constructed in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual.	N/A	The cross-fall of the property is about 0.3m and therefore, any future excavation and/or filling will be minimal and not require any batters and berms.

Performance outcomes	Acceptable outcomes	Complies	Comments
	A07.6 Retaining walls have a maximum height of 1.5 metres and are designed and constructed in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development manual.	✓	The cross-fall of the property is about 0.3m and therefore, any future excavation and/or filling will be minimal and with minimal requirement for retaining walls.
	A07.7 Excavation or filling at any point on a lot is to include measures that protect trees at the foot or top of cut or fill batters by the use of appropriate retaining methods and sensitive earth removal or placement and in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development manual.	✓	Can be conditioned to comply.
For assessable development			
Transport network			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO8 The development has access to a transport network of adequate standard to provide for the safe and efficient movement of vehicles, pedestrians and cyclists.	AO8.1 Vehicle access, crossovers, road geometry, pavement, utilities and landscaping to the frontage/s of the site are designed and constructed in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development manual.	✓	Can be conditioned to comply. The vehicle access, crossovers, road geometry, pavement, utilities and existing landscaping to the frontage of the site can be designed and constructed in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 - FNQROC Regional Development manual.
	AO8.2 Development provides footpath pavement treatments in accordance with Planning Scheme Policy 9 – Footpath Paving.	N/A	N/A
Public infrastructure			
PO9 The design, construction and provision of any infrastructure that is to be dedicated to Council is cost effective over its life cycle and incorporates provisions to minimise adverse impacts.	AO9 Development is in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual.	N/A	No new infrastructure is required as part of the proposed development.
Stormwater quality			

Performance outcomes	Acceptable outcomes	Complies	Comments
<p>PO10 Development has a non-worsening effect on the site and surrounding land and is designed to:</p> <ul style="list-style-type: none"> (a) optimise the interception, retention and removal of waterborne pollutants, prior to the discharge to receiving waters; (b) protect the environmental values of waterbodies affected by the development, including upstream, on-site and downstream waterbodies; (c) achieve specified water quality objectives; (d) minimise flooding; (e) maximise the use of natural channel design principles; (f) maximise community benefit; and (g) minimise risk to public safety. 	<p>AO10.1 The following reporting is prepared for all Material change of use or Reconfiguring a lot proposals:</p> <ul style="list-style-type: none"> (a) a Stormwater Management Plan and Report that meets or exceeds the standards of design and construction set out in the Queensland Urban Drainage Manual (QUDM) and the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual; and (b) an Erosion and Sediment Control Plan that meets or exceeds the Soil Erosion and Sedimentation Control Guidelines (Institute of Engineers Australia), including: <ul style="list-style-type: none"> (i) drainage control; (ii) erosion control; (iii) sediment control; and (iv) water quality outcomes. 	✓	Can be conditioned to comply.

Performance outcomes	Acceptable outcomes	Complies	Comments
	<p>AO10.2</p> <p>For development on land greater than 2,500m² or that result in more than 5 lots or more than 5 dwellings or accommodation units, a Stormwater Quality Management Plan and Report prepared and certified by a suitably qualified design engineer (RPEQ) is prepared that demonstrates that the development:</p> <ul style="list-style-type: none"> (a) meets or exceeds the standards of design and construction set out in the Urban Stormwater Quality Planning Guideline and the Queensland Water Quality Guideline; (b) is consistent with any local area stormwater water management planning; (c) accounts for development type, construction phase, local climatic conditions and design objectives; and (d) provides for stormwater quality treatment measures reflecting land use constraints, such as soil type, landscape features (including landform), nutrient hazardous areas, acid sulfate soil and rainfall erosivity. 	N/A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
PO11 Storage areas for stormwater detention and retention: (a) protect or enhance the environmental values of receiving waters; (b) achieve specified water quality objectives; (c) where possible, provide for recreational use; (d) maximise community benefit; and (e) minimise risk to public safety.	AO11 No acceptable outcome is provided.	N/A	N/A
Excavation or filling			
PO12 Traffic generated by filling or excavation does not impact on the amenity of the surrounding area.	AO12.1 Haul routes used for transportation of fill to or from the site only use major roads and avoid residential areas.	N/A	N/A -minimal filling or excavation is required as part of the proposed development.
	AO12.2 Transportation of fill to or from the site does not occur: (a) within peak traffic times; and (b) before 7am or after 6pm Monday to Friday; (c) before 7am or after 1pm Saturdays; and (d) on Sundays or Public Holidays.	N/A	N/A

Performance outcomes	Acceptable outcomes	Complies	Comments
PO13 Air pollutants, dust and sediment particles from excavation or filling, do not cause significant environmental harm or nuisance impacts.	AO13.1 Dust emissions do not extend beyond the boundary of the site.	N/A	N/A
	AO13.2 No other air pollutants, including odours, are detectable at the boundary of the site.	N/A	N/A
	AO13.3 A management plan for control of dust and air pollutants is prepared and implemented.	N/A	N/A
PO14 Access to the premises (including driveways and paths) does not have an adverse impact on: (a) safety; (b) drainage; (c) visual amenity; and (d) privacy of adjoining premises.	AO14 Access to the premises (including all works associated with the access): (a) must follow as close as possible to the existing contours; (b) be contained within the premises and not the road reserve, and (c) are designed and constructed in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development manual.	N/A	N/A
Weed and pest management			
PO15 Development prevents the spread of weeds, seeds or other pests into clean areas or away from infested areas.	AO15 No acceptable outcome is provided.	N/A	N/A
Contaminated land			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO16 Development is located and designed to ensure that users and nearby sensitive land uses are not exposed to unacceptable levels of contaminants	AO16 Development is located where: <ul style="list-style-type: none"> (a) soils are not contaminated by pollutants which represent a health or safety risk to users; or (b) contaminated soils are remediated prior to plan sealing, operational works permit, or issuing of building works permit. 	N/A	N/A
Fire services in developments accessed by common private title			
PO17 Fire hydrants are located in positions that will enable fire services to access water safely, effectively and efficiently.	AO17.1 Fire hydrants are located in accessways or private roads held in common private title at a maximum spacing of: <ul style="list-style-type: none"> (a) 120 metres for residential development; and (b) 90 metres for any other development. 	✓	The existing town reticulated water supply with street hydrant.
	AO17.2 Fire hydrants are located at all intersections of accessways or private roads held in common private title.	N/A	N/A

DA Form 1 – Development application details

Approved form (version 1.1 effective 22 JUNE 2018) made under section 282 of the Planning Act 2016.

This form **must** be used to make a development application **involving code assessment or impact assessment**, except when applying for development involving building work.

For a development application involving **building work only**, use *DA Form 2 – Building work details*.

For a development application involving **building work associated with any other type of assessable development (i.e. material change of use, operational work or reconfiguring a lot)**, use this form (*DA Form 1*) and parts 4 to 6 of *DA Form 2 – Building work details*.

Unless stated otherwise, all parts of this form **must** be completed in full and all required supporting information **must** accompany the development application.

One or more additional pages may be attached as a schedule to this development application if there is insufficient space on the form to include all the necessary information.

This form and any other form relevant to the development application must be used to make a development application relating to strategic port land and Brisbane core port land under the *Transport Infrastructure Act 1994*, and airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*. For the purpose of assessing a development application relating to strategic port land and Brisbane core port land, any reference to a planning scheme is taken to mean a land use plan for the strategic port land, Brisbane port land use plan for Brisbane core port land, or a land use plan for airport land.

Note: All terms used in this form have the meaning given under the *Planning Act 2016*, the *Planning Regulation 2017*, or the *Development Assessment Rules (DA Rules)*.

PART 1 – APPLICANT DETAILS

1) Applicant details	
Applicant name(s) (individual or company full name)	Mulungu Aboriginal Corporation C/- Northern Building Approvals
Contact name (only applicable for companies)	Kenton Byrne
Postal address (P.O. Box or street address)	3b Margherita Close
Suburb	Mareeba
State	QLD
Postcode	4880
Country	Australia
Contact number	0447 865 265
Email address (non-mandatory)	kentonstella@bigpond.com
Mobile number (non-mandatory)	
Fax number (non-mandatory)	
Applicant's reference number(s) (if applicable)	DA/18/0001

2) Owner's consent
2.1) Is written consent of the owner required for this development application?
<input type="checkbox"/> Yes – the written consent of the owner(s) is attached to this development application
<input type="checkbox"/> No – proceed to 3)

PART 2 – LOCATION DETAILS

3) Location of the premises (complete 3.1) or 3.2), and 3.3) as applicable)

Note: Provide details below and attach a site plan for any or all premises part of the development application. For further information, see [DA Forms Guide: Relevant plans](#).

3.1) Street address and lot on plan

☒ Street address **AND** lot on plan (all lots must be listed), **or**

☐ Street address **AND** lot on plan for an adjoining or adjacent property of the premises (appropriate for development in water but adjoining or adjacent to land e.g. jetty, pontoon; all lots must be listed).

a)	Unit No.	Street No.	Street Name and Type	Suburb
	2	14	Sutherland Street	Mareeba
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)
	4880	4	RP717124	Mareeba Shire Council
b)	Unit No.	Street No.	Street Name and Type	Suburb
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)

3.2) Coordinates of premises (appropriate for development in remote areas, over part of a lot or in water not adjoining or adjacent to land e.g. channel dredging in Moreton Bay)

Note: Place each set of coordinates in a separate row. Only one set of coordinates is required for this part.

☐ Coordinates of premises by longitude and latitude

Longitude(s)	Latitude(s)	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

☐ Coordinates of premises by easting and northing

Easting(s)	Northing(s)	Zone Ref.	Datum	Local Government Area(s) (if applicable)
		<input type="checkbox"/> 54 <input type="checkbox"/> 55 <input type="checkbox"/> 56	<input type="checkbox"/> WGS84 <input type="checkbox"/> GDA94 <input type="checkbox"/> Other:	

3.3) Additional premises

☐ Additional premises are relevant to this development application and their details have been attached in a schedule to this application

☒ Not required

4) Identify any of the following that apply to the premises and provide any relevant details

☐ In or adjacent to a water body or watercourse or in or above an aquifer

Name of water body, watercourse or aquifer:

☐ On strategic port land under the *Transport Infrastructure Act 1994*

Lot on plan description of strategic port land:

Name of port authority for the lot:

☐ In a tidal area

Name of local government for the tidal area (if applicable):

Name of port authority for tidal area (if applicable):

☐ On airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*

Name of airport:

<input type="checkbox"/> Listed on the Environmental Management Register (EMR) under the <i>Environmental Protection Act 1994</i>	
EMR site identification:	
<input type="checkbox"/> Listed on the Contaminated Land Register (CLR) under the <i>Environmental Protection Act 1994</i>	
CLR site identification:	

5) Are there any existing easements over the premises?

Note: Easement uses vary throughout Queensland and are to be identified correctly and accurately. For further information on easements and how they may affect the proposed development, see [DA Forms Guide](#).

- ☐ Yes – All easement locations, types and dimensions are included in plans submitted with this development application
- ☐ No

PART 3 – DEVELOPMENT DETAILS

Section 1 – Aspects of development

6.1) Provide details about the first development aspect

a) What is the type of development? *(tick only one box)*

- ☒ Material change of use ☐ Reconfiguring a lot ☐ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☒ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☐ Code assessment ☒ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

Proposed Open Carport Structure to Existing Community Care Centre Development

e) Relevant plans

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms guide: Relevant plans](#).

- ☒ Relevant plans of the proposed development are attached to the development application

6.2) Provide details about the second development aspect

a) What is the type of development? *(tick only one box)*

- ☐ Material change of use ☐ Reconfiguring a lot ☐ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☐ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☐ Code assessment ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal *(e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):*

e) Relevant plans

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).

- ☐ Relevant plans of the proposed development are attached to the development application

6.3) Additional aspects of development

- ☐ Additional aspects of development are relevant to this development application and the details for these aspects that would be required under Part 3 Section 1 of this form have been attached to this development application
- ☐ Not required

Section 2 – Further development details

7) Does the proposed development application involve any of the following?

Material change of use	<input checked="" type="checkbox"/> Yes – complete division 1 if assessable against a local planning instrument
Reconfiguring a lot	<input type="checkbox"/> Yes – complete division 2
Operational work	<input type="checkbox"/> Yes – complete division 3
Building work	<input type="checkbox"/> Yes – complete DA Form 2 – Building work details

Division 1 – Material change of use

Note: This division is only required to be completed if any part of the development application involves a material change of use assessable against a local planning instrument.

8.1) Describe the proposed material change of use

Provide a general description of the proposed use	Provide the planning scheme definition (include each definition in a new row)	Number of dwelling units (if applicable)	Gross floor area (m ²) (if applicable)
Carpark	Community Care Centre	N/A	448m ²

8.2) Does the proposed use involve the use of existing buildings on the premises?

<input checked="" type="checkbox"/> Yes		
<input type="checkbox"/> No		

Division 2 – Reconfiguring a lot

Note: This division is only required to be completed if any part of the development application involves reconfiguring a lot.

9.1) What is the total number of existing lots making up the premises?

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9.2) What is the nature of the lot reconfiguration? (tick all applicable boxes)

<input type="checkbox"/> Subdivision (complete 10))	<input type="checkbox"/> Dividing land into parts by agreement (complete 11))
<input type="checkbox"/> Boundary realignment (complete 12))	<input type="checkbox"/> Creating or changing an easement giving access to a lot from a construction road (complete 13))

10) Subdivision

10.1) For this development, how many lots are being created and what is the intended use of those lots:

Intended use of lots created	Residential	Commercial	Industrial	Other, please specify:
Number of lots created				

10.2) Will the subdivision be staged?

<input type="checkbox"/> Yes – provide additional details below	
<input type="checkbox"/> No	
How many stages will the works include?	
What stage(s) will this development application apply to?	

11) Dividing land into parts by agreement – how many parts are being created and what is the intended use of the parts?

Intended use of parts created	Residential	Commercial	Industrial	Other, please specify:
Number of parts created				

12) Boundary realignment

12.1) What are the current and proposed areas for each lot comprising the premises?

Current lot		Proposed lot	
Lot on plan description	Area (m ²)	Lot on plan description	Area (m ²)

12.2) What is the reason for the boundary realignment?

--

13) What are the dimensions and nature of any existing easements being changed and/or any proposed easement? (attach schedule if there are more than two easements)

Existing or proposed?	Width (m)	Length (m)	Purpose of the easement? (e.g. pedestrian access)	Identify the land/lot(s) benefitted by the easement

Division 3 – Operational work

Note: This division is only required to be completed if any part of the development application involves operational work.

14.1) What is the nature of the operational work?

- | | | |
|--|-------------------------------------|--|
| <input type="checkbox"/> Road work | <input type="checkbox"/> Stormwater | <input type="checkbox"/> Water infrastructure |
| <input type="checkbox"/> Drainage work | <input type="checkbox"/> Earthworks | <input type="checkbox"/> Sewage infrastructure |
| <input type="checkbox"/> Landscaping | <input type="checkbox"/> Signage | <input type="checkbox"/> Clearing vegetation |
| <input type="checkbox"/> Other – please specify: <table border="1" style="display: inline-table; width: 300px; height: 20px;"></table> | | |

14.2) Is the operational work necessary to facilitate the creation of new lots? (e.g. subdivision)

☐ Yes – specify number of new lots:

☐ No

14.3) What is the monetary value of the proposed operational work? (include GST, materials and labour)

\$

PART 4 – ASSESSMENT MANAGER DETAILS

15) Identify the assessment manager(s) who will be assessing this development application

Mareeba Shire Council

16) Has the local government agreed to apply a superseded planning scheme for this development application?

- ☐ Yes – a copy of the decision notice is attached to this development application
- ☐ Local government is taken to have agreed to the superseded planning scheme request – relevant documents attached
- ☒ No

PART 5 – REFERRAL DETAILS

17) Do any aspects of the proposed development require referral for any referral requirements?

Note: A development application will require referral if prescribed by the Planning Regulation 2017.

☒ No, there are no referral requirements relevant to any development aspects identified in this development application – proceed to Part 6

Matters requiring referral to the **Chief Executive of the Planning Regulation 2017:**

- ☐ Clearing native vegetation
- ☐ Contaminated land (*unexploded ordnance*)
- ☐ Environmentally relevant activities (ERA) (*only if the ERA have not been devolved to a local government*)
- ☐ Fisheries – aquaculture
- ☐ Fisheries – declared fish habitat area
- ☐ Fisheries – marine plants
- ☐ Fisheries – waterway barrier works
- ☐ Hazardous chemical facilities
- ☐ Queensland heritage place (*on or near a Queensland heritage place*)
- ☐ Infrastructure – designated premises
- ☐ Infrastructure – state transport infrastructure
- ☐ Infrastructure – state transport corridors and future state transport corridors
- ☐ Infrastructure – state-controlled transport tunnels and future state-controlled transport tunnels
- ☐ Infrastructure – near a state-controlled road intersection
- ☐ On Brisbane core port land near a State transport corridor or future State transport corridor
- ☐ On Brisbane core port land – ERA
- ☐ On Brisbane core port land – tidal works or work in a coastal management district
- ☐ On Brisbane core port land – hazardous chemical facility
- ☐ On Brisbane core port land – taking or interfering with water
- ☐ On Brisbane core port land – referable dams
- ☐ On Brisbane core port land - fisheries
- ☐ Land within Port of Brisbane's port limits
- ☐ SEQ development area
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – tourist activity or sport and recreation activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – community activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – indoor recreation
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – urban activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – combined use
- ☐ Tidal works or works in a coastal management district
- ☐ Reconfiguring a lot in a coastal management district or for a canal
- ☐ Erosion prone area in a coastal management district
- ☐ Urban design
- ☐ Water-related development – taking or interfering with water
- ☐ Water-related development – removing quarry material (*from a watercourse or lake*)
- ☐ Water-related development – referable dams
- ☐ Water-related development – construction of new levees or modification of existing levees (*category 3 levees only*)
- ☐ Wetland protection area

Matters requiring referral to the **local government:**

- ☐ Airport land
- ☐ Environmentally relevant activities (ERA) (*only if the ERA have been devolved to local government*)
- ☐ Local heritage places

Matters requiring referral to the chief executive of the distribution entity or transmission entity : <input type="checkbox"/> Electricity infrastructure
Matters requiring referral to: <ul style="list-style-type: none"> • The Chief executive of the holder of the licence, if not an individual • The holder of the licence, if the holder of the licence is an individual <input type="checkbox"/> Oil and gas infrastructure
Matters requiring referral to the Brisbane City Council : <input type="checkbox"/> Brisbane core port land
Matters requiring referral to the Minister under the Transport Infrastructure Act 1994 : <input type="checkbox"/> Brisbane core port land (inconsistent with Brisbane port LUP for transport reasons) <input type="checkbox"/> Strategic port land
Matters requiring referral to the relevant port operator : <input type="checkbox"/> Land within Port of Brisbane's port limits (below high-water mark)
Matters requiring referral to the Chief Executive of the relevant port authority : <input type="checkbox"/> Land within limits of another port (below high-water mark)
Matters requiring referral to the Gold Coast Waterways Authority : <input type="checkbox"/> Tidal works, or work in a coastal management district in Gold Coast waters
Matters requiring referral to the Queensland Fire and Emergency Service : <input type="checkbox"/> Tidal works marina (<i>more than six vessel berths</i>)

18) Has any referral agency provided a referral response for this development application?		
<input type="checkbox"/> Yes – referral response(s) received and listed below are attached to this development application <input type="checkbox"/> No		
Referral requirement	Referral agency	Date of referral response
Identify and describe any changes made to the proposed development application that was the subject of the referral response and the development application the subject of this form, or include details in a schedule to this development application (<i>if applicable</i>).		

PART 6 – INFORMATION REQUEST

19) Information request under Part 3 of the DA Rules
<input checked="" type="checkbox"/> I agree to receive an information request if determined necessary for this development application <input type="checkbox"/> I do not agree to accept an information request for this development application
Note: By not agreeing to accept an information request I, the applicant, acknowledge: <ul style="list-style-type: none"> • that this development application will be assessed and decided based on the information provided when making this development application and the assessment manager and any referral agencies relevant to the development application are not obligated under the DA Rules to accept any additional information provided by the applicant for the development application unless agreed to by the relevant parties • Part 3 of the DA Rules will still apply if the application is an application listed under section 11.3 of the DA Rules. Further advice about information requests is contained in the DA Forms Guide .

PART 7 – FURTHER DETAILS

20) Are there any associated development applications or current approvals? (e.g. a preliminary approval)

- ☐ Yes – provide details below or include details in a schedule to this development application
☒ No

List of approval/development application references	Reference number	Date	Assessment manager
<input type="checkbox"/> Approval			
<input type="checkbox"/> Development application			
<input type="checkbox"/> Approval			
<input type="checkbox"/> Development application			

21) Has the portable long service leave levy been paid? (only applicable to development applications involving building work or operational work)

- ☐ Yes – a copy of the receipted QLeave form is attached to this development application
☐ No – I, the applicant will provide evidence that the portable long service leave levy has been paid before the assessment manager decides the development application. I acknowledge that the assessment manager may give a development approval only if I provide evidence that the portable long service leave levy has been paid
☒ Not applicable (e.g. building and construction work is less than \$150,000 excluding GST)

Amount paid	Date paid (dd/mm/yy)	QLeave levy number
\$		

22) Is this development application in response to a show cause notice or required as a result of an enforcement notice?

- ☐ Yes – show cause or enforcement notice is attached
☐ No

23) Further legislative requirements

Environmentally relevant activities

23.1) Is this development application also taken to be an application for an environmental authority for an **Environmentally Relevant Activity (ERA)** under section 115 of the *Environmental Protection Act 1994*?

- ☐ Yes – the required attachment (form ESR/2015/1791) for an application for an environmental authority accompanies this development application, and details are provided in the table below
☒ No

Note: Application for an environmental authority can be found by searching “ESR/2015/1791” as a search term at www.qld.gov.au. An ERA requires an environmental authority to operate. See www.business.qld.gov.au for further information.

Proposed ERA number:		Proposed ERA threshold:	
Proposed ERA name:			

- ☐ Multiple ERAs are applicable to this development application and the details have been attached in a schedule to this development application.

Hazardous chemical facilities

23.2) Is this development application for a **hazardous chemical facility**?

- ☐ Yes – Form 69: Notification of a facility exceeding 10% of schedule 15 threshold is attached to this development application
☒ No

Note: See www.business.qld.gov.au for further information about hazardous chemical notifications.

Clearing native vegetation

23.3) Does this development application involve **clearing native vegetation** that requires written confirmation that the chief executive of the *Vegetation Management Act 1999* is satisfied the clearing is for a relevant purpose under section 22A of the *Vegetation Management Act 1999*?

☐ Yes – this development application includes written confirmation from the chief executive of the *Vegetation Management Act 1999* (s22A determination)

☒ No

Note: 1. Where a development application for operational work or material change of use requires a s22A determination and this is not included, the development application is prohibited development.

2. See <https://www.qld.gov.au/environment/land/vegetation/applying> for further information on how to obtain a s22A determination.

Environmental offsets

23.4) Is this development application taken to be a prescribed activity that may have a significant residual impact on a **prescribed environmental matter** under the *Environmental Offsets Act 2014*?

☐ Yes – I acknowledge that an environmental offset must be provided for any prescribed activity assessed as having a significant residual impact on a prescribed environmental matter

☒ No

Note: The environmental offset section of the Queensland Government's website can be accessed at www.qld.gov.au for further information on environmental offsets.

Koala conservation

23.5) Does this development application involve a material change of use, reconfiguring a lot or operational work within an assessable development area under Schedule 10, Part 10 of the Planning Regulation 2017?

☐ Yes

☒ No

Note: See guidance materials at www.des.qld.gov.au for further information.

Water resources

23.6) Does this development application involve **taking or interfering with underground water through an artesian or subartesian bore, taking or interfering with water in a watercourse, lake or spring, or taking overland flow water under the Water Act 2000**?

☐ Yes – the relevant template is completed and attached to this development application and I acknowledge that a relevant authorisation or licence under the *Water Act 2000* may be required prior to commencing development

☒ No

Note: Contact the Department of Natural Resources, Mines and Energy at www.dnrme.qld.gov.au for further information.

DA templates are available from <https://planning.dsdmip.qld.gov.au/>. If the development application involves:

- Taking or interfering with underground water through an artesian or subartesian bore: complete DA Form 1 Template 1
- Taking or interfering with water in a watercourse, lake or spring: complete DA Form 1 Template 2
- Taking overland flow water: complete DA Form 1 Template 3.

Waterway barrier works

23.7) Does this application involve **waterway barrier works**?

☐ Yes – the relevant template is completed and attached to this development application

☒ No

DA templates are available from <https://planning.dsdmip.qld.gov.au/>. For a development application involving waterway barrier works, complete DA Form 1 Template 4.

Marine activities

23.8) Does this development application involve **aquaculture, works within a declared fish habitat area or removal, disturbance or destruction of marine plants**?

☐ Yes – an associated resource allocation authority is attached to this development application, if required under the *Fisheries Act 1994*

☒ No

Note: See guidance materials at www.daf.qld.gov.au for further information.

Quarry materials from a watercourse or lake

23.9) Does this development application involve the **removal of quarry materials from a watercourse or lake** under the *Water Act 2000*?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
☒ No

Note: Contact the Department of Natural Resources, Mines and Energy at www.dnrme.qld.gov.au and www.business.qld.gov.au for further information.

Quarry materials from land under tidal waters

23.10) Does this development application involve the **removal of quarry materials from land under tidal water** under the *Coastal Protection and Management Act 1995*?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
☒ No

Note: Contact the Department of Environment and Science at www.des.qld.gov.au for further information.

Referable dams

23.11) Does this development application involve a **referable dam** required to be failure impact assessed under section 343 of the *Water Supply (Safety and Reliability) Act 2008* (the Water Supply Act)?

- ☐ Yes – the 'Notice Accepting a Failure Impact Assessment' from the chief executive administering the Water Supply Act is attached to this development application
☒ No

Note: See guidance materials at www.dnrme.qld.gov.au for further information.

Tidal work or development within a coastal management district

23.12) Does this development application involve **tidal work or development in a coastal management district**?

- ☐ Yes – the following is included with this development application:
☐ Evidence the proposal meets the code for assessable development that is prescribed tidal work (*only required if application involves prescribed tidal work*)
☐ A certificate of title
☒ No

Note: See guidance materials at www.des.qld.gov.au for further information.

Queensland and local heritage places

23.13) Does this development application propose development on or adjoining a place entered in the **Queensland heritage register** or on a place entered in a local government's **Local Heritage Register**?

- ☐ Yes – details of the heritage place are provided in the table below
☒ No

Note: See guidance materials at www.des.qld.gov.au for information requirements regarding development of Queensland heritage places.

Name of the heritage place:		Place ID:	
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Brothels

23.14) Does this development application involve a **material change of use for a brothel**?

- ☐ Yes – this development application demonstrates how the proposal meets the code for a development application for a brothel under Schedule 3 of the *Prostitution Regulation 2014*
☒ No

Decision under section 62 of the Transport Infrastructure Act 1994

23.15) Does this development application involve new or changed access to a state-controlled road?

- ☐ Yes - this application will be taken to be an application for a decision under section 62 of the *Transport Infrastructure Act 1994* (subject to the conditions in section 75 of the *Transport Infrastructure Act 1994* being satisfied)
☒ No

PART 8 – CHECKLIST AND APPLICANT DECLARATION

24) Development application checklist

I have identified the assessment manager in question 15 and all relevant referral requirement(s) in question 17 <i>Note: See the Planning Regulation 2017 for referral requirements</i>	<input checked="" type="checkbox"/> Yes
If building work is associated with the proposed development, Parts 4 to 6 of <i>DA Form 2 – Building work details</i> have been completed and attached to this development application	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> Not applicable
Supporting information addressing any applicable assessment benchmarks is with development application <i>Note: This is a mandatory requirement and includes any relevant templates under question 23, a planning report and any technical reports required by the relevant categorising instruments (e.g. local government planning schemes, State Planning Policy, State Development Assessment Provisions). For further information, see DA Forms Guide: Planning Report Template.</i>	<input checked="" type="checkbox"/> Yes
Relevant plans of the development are attached to this development application <i>Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see DA Forms Guide: Relevant plans.</i>	<input checked="" type="checkbox"/> Yes
The portable long service leave levy for QLeave has been paid, or will be paid before a development permit is issued (<i>see 21</i>))	<input type="checkbox"/> Yes <input checked="" type="checkbox"/> Not applicable

25) Applicant declaration

<input checked="" type="checkbox"/> By making this development application, I declare that all information in this development application is true and correct <input checked="" type="checkbox"/> Where an email address is provided in Part 1 of this form, I consent to receive future electronic communications from the assessment manager and any referral agency for the development application where written information is required or permitted pursuant to sections 11 and 12 of the <i>Electronic Transactions Act 2001</i> <i>Note: It is unlawful to intentionally provide false or misleading information.</i>
<p>Privacy – Personal information collected in this form will be used by the assessment manager and/or chosen assessment manager, any relevant referral agency and/or building certifier (including any professional advisers which may be engaged by those entities) while processing, assessing and deciding the development application. All information relating to this development application may be available for inspection and purchase, and/or published on the assessment manager's and/or referral agency's website.</p> <p>Personal information will not be disclosed for a purpose unrelated to the <i>Planning Act 2016</i>, Planning Regulation 2017 and the DA Rules except where:</p> <ul style="list-style-type: none"> such disclosure is in accordance with the provisions about public access to documents contained in the <i>Planning Act 2016</i> and the Planning Regulation 2017, and the access rules made under the <i>Planning Act 2016</i> and Planning Regulation 2017; or required by other legislation (including the <i>Right to Information Act 2009</i>); or otherwise required by law. <p>This information may be stored in relevant databases. The information collected will be retained as required by the <i>Public Records Act 2002</i>.</p>

PART 9 – FOR OFFICE USE ONLY

Date received:	<input type="text"/>	Reference number(s):	<input type="text"/>
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Notification of engagement of alternative assessment manager

Prescribed assessment manager	<input type="text"/>
Name of chosen assessment manager	<input type="text"/>
Date chosen assessment manager engaged	<input type="text"/>
Contact number of chosen assessment manager	<input type="text"/>

Relevant licence number(s) of chosen assessment manager	
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QLeave notification and payment

Note: For completion by assessment manager if applicable

Description of the work	
QLeave project number	
Amount paid (\$)	
Date paid	
Date receipted form sighted by assessment manager	
Name of officer who sighted the form	