

DEVELOPMENT APPLICATION FOR A DEVELOPMENT
PERMIT FOR:

COMBINED APPLICATION: RECONFIGURING A LOT
(1 LOT INTO 44 LOTS, NEW ROAD, & ACCESS &
SERVICES EASEMENTS) AND MATERIAL CHANGE OF
USE (DUAL OCCUPANCIES)

on behalf of
FS Developments Pty Ltd

at
2-18 Haren Street, Mareeba QLD

on
Lot 1 on SP298397





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1.0 INTRODUCTION

This town planning report has been prepared on behalf of the Applicant, FS Developments Pty Ltd, in support of a Development Application seeking a Development Permit for Reconfiguring a Lot (1 into 44 Lots, New Road, and Access and Services Easements) and Material Change of Use for Dual Occupancies on land at 2-18 Haren Street, Mareeba, formally described as Lot 1 on SP298397.

To assist in Council's determination of this development application, this planning report covers the following matters:

- Section 2:- Subject site description.
- Section 3:- A detailed description of the development proposal.
- Section 4:- A review of the relevant legislation provisions.
- Section 5:- An assessment of the proposal against the relevant code provisions of the *Mareeba Shire Council Planning Scheme 2016*.
- Section 6:- Conclusion.

The development application is made in accordance with section 51 of the *Planning Act 2016* and contains the mandatory supporting information specified in the applicable DA Form, included in **Appendix A**.

In accordance with section 51(2) of the *Planning Act 2016*, as the Applicant is not the registered owner of the premises, landowner's consent is included in **Appendix B**.

Under the *Mareeba Shire Council Planning Scheme 2016*, the application is subject to code assessment and therefore public notification is not required to be undertaken.

This application triggers referral to the State Assessment Referral Agency (SARA).



2.0 THE SUBJECT SITE

The site is located at 2-18 Haren Street, Mareeba, formally described as Lot 1 on SP298397, comprising a total area of 2.893ha with multiple road frontages to Haren Street, Mareeba Connection Road and Antonio Drive. Access is currently afforded from Haren Street. *Figure 1* below demonstrates the extent of the site and surrounding development.



Figure 1: Aerial image of subject site (Source: Queensland Globe 2026)

The site is approximately 180m from the outskirts of the Mareeba centre area and is currently unimproved with sparse vegetation. The site is generally surrounded by residential development and open space. The slope aspect gently falls northeast towards an existing drainage path adjacent Antonio Drive.

The site is within the Low Density Residential Zone for the purpose of the *Mareeba Shire Council Planning Scheme 2016*. An extract of the zoning map is shown in *Figure 2* below.



Figure 2: Extract from Zoning Mapping (Source: Mareeba Shire Council)



The land contains sewer infrastructure that traverses the eastern boundary from Mareeba Connection Road within Easement A on SP298397. Connection to Council's reticulated services is readily available.

The certificate of Title confirming ownership of the subject land by Signature Care Land Holdings Pty Ltd is included in **Appendix B**. The certificate of Title also identifies that the subject land is encumbered by Easement A on SP298397 (in gross to Council for sewer infrastructure purposes) which will not be impacted as a result of the proposal.

A copy of Survey Plan – SP298397 detailing the existing lot configuration is included in **Appendix B**.



3.0 THE PROPOSAL

Approval of the Development Application will authorise a Development Permit for Reconfiguring a Lot (1 Lot into 44 Lots, New Road, and Access and Services Easements) and Material Change of Use for Dual Occupancies on land at 2-18 Haren Street, Mareeba, formally described as Lot 1 on SP298397.

The proposal seeks to unlock critical residential land that directly supports Mareeba's growing housing demand and ease long term waitlisting for residential lots. The application seeks to approve the creation of 44 residential lots, to then further permit the development of dual occupancies over a selection of 9 of the new lots.

The proposed reconfiguration is detailed within Proposal Plan 34692/003C included with **Appendix F**, with an extract provided within *Figure 3* below.



Figure 3: Extract of Proposal Plan 34692/003 Rev C (Source: Brazier Motti 2026)

The proposal will result in the creation of 44 residential lots and new road via Haren Street, with lot areas ranging from 450m² to 925m². The lots against the easternmost site boundary are encumbered by the existing services Easement A on SP29397 as depicted in the Proposal Plan. Proposed Lots 18 and 22 are further encumbered by new access easements benefitting proposed Lot 17 and Lots 21 and 23, respectively. Proposed Lot 33 contains a new drainage easement. The proposed reconfiguration results in a total of six (6) rear lots which are considered appropriate to maximise the utilisation of land given the unique configuration of the existing site boundaries.

As demonstrated in the engineering report prepared by Neon Consulting, copy included in **Appendix E**, the proposal can be effectively connected to essential utility services and manage stormwater without impact surrounding land.

A Building Setback Plan is provided for the proposed development to vary the setback distances for future dwellings on site from those prescribed with the Queensland Development Code (QDC). The variation will ensure that each lot can achieve its optimal development potential. The minor inconsistencies with the QDC provisions will not adversely impact upon the liveability or visual amenity of the development and will result in acceptable streetscapes consistent with that expected for the Low Density Residential Zone. The approval of the Building Setback Plan ensures streamlining the building



approval process. Figure 4 below provides an extract of the Building Setback Plan, with a copy included in **Appendix G**.

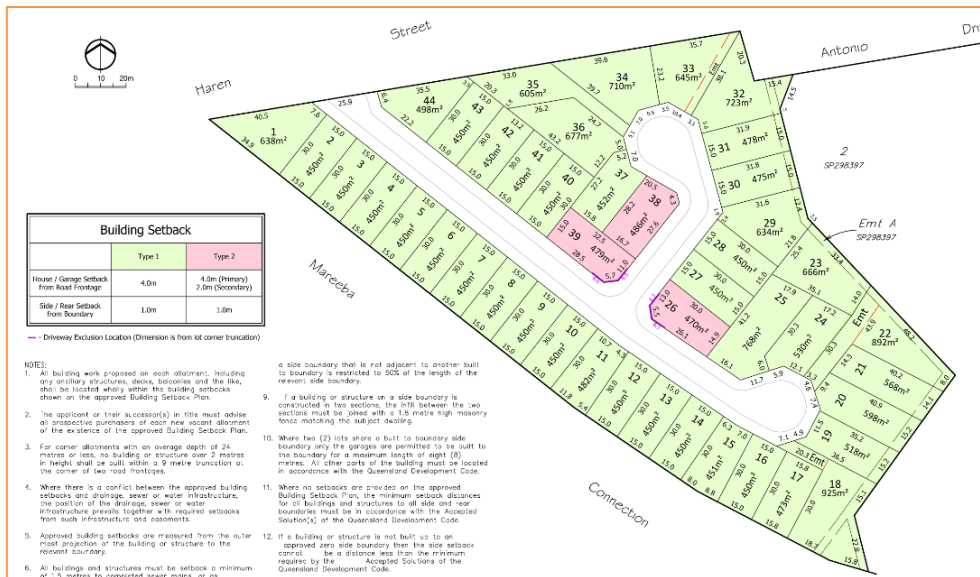


Figure 4: Extract of Building Setback Plan 34692/005 Rev B (Source: Brazier Motti 2026)

The proposal includes the development of dual occupancies within 9 of the proposed lots (Lots 1, 24, 25, 29, 32, 33, 34, 36, and 38), refer to the extract included in Figure 5 below. The dual occupancies consist of 5 dwelling types containing a variation of 2 bedroom or 3 bedroom designs, and these design plans are included in **Appendix H**. Each dual occupancy design is responsive to site constraints and compliant with the proposed Building Setback Plan.

Overall, the proposal is considered to be compatible with the subject site and surrounding locality and demonstrates consistency with the land allocation identified for the Low Density Residential Zone of the *Mareeba Shire Council Planning Scheme 2016*.

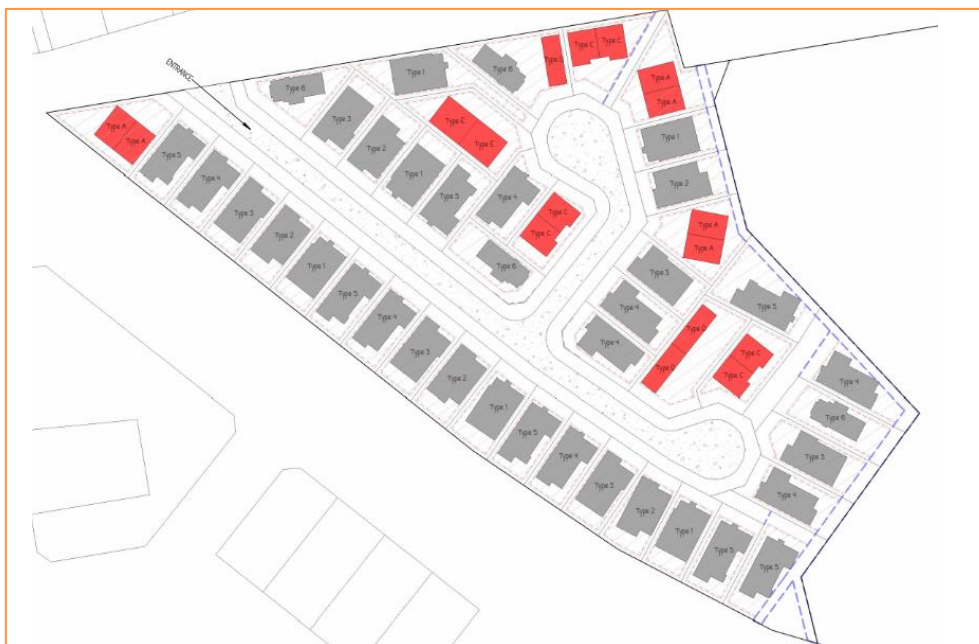


Figure 5: Extract of Dwelling Masterplan identifying dual occupancy allocation (Source: EDR Building Designs 2026)



4.0 RELEVANT LEGISLATION

4.1 COMMONWEALTH LEGISLATION

The application is not subject to assessment against Commonwealth legislation. It is not anticipated that development of this land will trigger assessment against the *Environmental Protection and Biodiversity Conservation Act 1999 (EPBC)*, as it is not anticipated that the development will significantly impact upon a matter of national environmental significance.

4.2 THE PLANNING ACT 2016

The *Planning Act 2016* provides the framework for coordinating local, regional and state planning. Given the nature of the development, the application requires assessment against this legislation.

4.3 STATE ASSESSMENT AND REFERRALS

State interest mapping reveals that the subject site is within 25m of a State transport corridor and the application will result in over 50 dwellings. Review of the proposed development against Schedule 10 of the *Planning Regulation 2017* confirms that the application requires referral to SARA for the following triggers:

- **10.9.4.1.1.1** | Aspect of development stated in schedule 20 that exceeds the threshold for development in a local government area 2.
- **10.9.4.2.1.1** | Reconfiguring a lot near a State transport corridor or that is a future State transport corridor where all or part of the premises are within 25m of a State transport corridor and the total number of lots is increased.
- **10.9.4.4.1.1** | Material change of use of premises near a State transport corridor or that is a future State transport corridor.

A copy of the response to State Code 1 is included in **Appendix C**.

4.4 STATE PLANNING POLICY

In accordance with section 2.1 – State Planning Policy (SPP) of the planning scheme, the Minister has identified that all aspects of the SPP have been integrated into the planning scheme. Hence, for the purposes of this development, we consider that assessment of the proposal against the provisions of the SPP is not required, and all relevant matters will be dealt with under the provisions of the planning scheme.

4.5 ASSESSMENT MANAGER AND PLANNING SCHEME

Mareeba Shire Council is nominated as the assessment manager for the application. The applicable planning scheme is the *Mareeba Shire Council Planning Scheme 2016* (Planning Scheme).

4.6 PUBLIC NOTIFICATION

The proposed development on the subject land does not require public notification.

4.7 OWNERS CONSENT

In accordance with section 51 of the *Planning Act 2016*, written consent is included in **Appendix B** as the Applicant is not the registered owner of the premises.



4.8 WALKABLE NEIGHBOURHOODS ASSESSMENT (SCHEDULE 12A PLANNING REGULATION)

In accordance with Schedule 12A of the *Planning Regulation 2017*, the proposed development is required to be assessed against the assessment benchmarks for reconfiguring a lot to which this schedule applies. The following sections demonstrate compliance with the identified benchmarks.

Connectivity

The site is bound by a State-controlled road, Haren Street, and vacant residential land. The resultant configuration is responsive of these features and maintains connectivity as far as practicable. Access to each lot is provided via New Road (cul-de-sacs) from Haren Street. Due to the hierarchical status of the surrounding transport network (State-controlled road) it is necessary to provide access via an Access Place and cul-de-sacs to maintain the safety and efficiency of the transport network. Footpaths can be conditioned to maintain pedestrian connectivity with the surrounding network where appropriate.

Maximum Length of Particular Blocks

The proposed reconfiguration does not contain any blocks with a length exceeding 250m.

Street Trees

Street trees can be provided in accordance with the FNQROC Development Manual (D9 Landscaping), following the completion of necessary operational works within the site. Street landscaping can be conditioned to comply.

Footpaths

The proposed reconfiguration will be accessed via an Access Place and footpaths can be conditioned to maintain pedestrian connectivity.

Parks and Other Areas of Open Space

The subject site is within 400m of an established park - Centenary Park.



5.0 THE PLANNING FRAMEWORK

5.1 MAREEBA SHIRE COUNCIL PLANNING SCHEME 2016

The Planning Scheme seeks to achieve outcomes through the identification of a number of overall outcomes, performance outcomes and acceptable solutions. Land identified within the planning scheme is divided into a number of zones. Zones are further identified within individual precincts and local plans. The Planning Scheme further identifies numerous overlay codes.

5.2 PLANNING SCHEME DESIGNATIONS

In accordance with the *Mareeba Shire Council Planning Scheme 2016* (Planning Scheme), the site is subject to the designations listed in *Table 1* below. These designations will assist in determining which tables of assessment are applicable to the proposed development on the subject site and therefore assist in the determination of the category of assessment and the codes applicable to the proposed development.

Table 1: Planning Scheme Designation

Type of Designation	Designation Applicability
Zone	Low Density Residential Zone
Airport Environs Overlay	Bird and bat strike zone – 8km
Bushfire Hazard Overlay	Potential Impact Buffer Medium potential bushfire intensity
Transport Infrastructure Overlay	Access Road (Haren, Constance, and Antonio Street) State Road (Mareeba Connection Road)

5.3 LEVEL OF ASSESSMENT, ASSESSABLE BENCHMARKS AND APPLICABLE CODES

The subject site is designated within Low Density Residential Zone and three (3) overlays.

Typically, a dual occupancy is accepted development subject to requirements within the Low Density Residential Zone, however as the proposed dual occupancies are within lots below 1,000m², this aspect is raised to code assessable development.

The relevant table of assessment within the Planning Scheme (Table 5.6.1) identifies the combined Reconfiguring a Lot and Material Change of Use as assessable development and is code assessable.

Furthermore, the proposed development will be assessed against the following codes:

- Low Density Residential Zone Code;
- Airport Environs Overlay Code;
- Bushfire Hazard Overlay Code;
- Transport Infrastructure Overlay Code;
- Accommodation Activities Code;
- Landscaping Code
- Parking and Access Code;
- Reconfiguring a Lot Code; and
- Works, services and infrastructure Code.

The following sections provide an assessment of the proposal against the relevant provisions of applicable codes:



5.4 ZONE CODE PROVISIONS

5.4.1 Low Density Residential Zone Code

In accordance with the *Mareeba Shire Council Planning Scheme 2016*, the site is contained within the Low Density Residential Zone.

The purpose of the Low Density Residential Zone Code is to provide for predominantly dwelling houses supported by community uses and small-scale services and facilities that cater for local residents.

Response

The proposal aligns with the purpose and outcomes of the Low Density Residential Zone Code in that it provides low density residential development. The site is ideally located and capable of being efficiently integrated into the surrounding environment and existing infrastructure networks.

Detailed assessment of the proposal is included in **Appendix D**.

5.5 OVERLAY CODES

5.5.1 Airport Environs Overlay Code

Overlay mapping identifies that the proposal requires assessment against the Airport Environs Overlay Code. The purpose of the code is to protect the current and ongoing operations of established airports, aerodromes and aviation infrastructure in Mareeba Shire.

Response

The proposal is for low density residential development and does not involve structures or any aspect that would interfere with the Mareeba airport or aircraft operations or encourage wildlife. Detailed assessment against the code is not necessary for this proposal.

5.5.2 Bushfire Hazard Overlay Code

Overlay mapping identifies that the proposal requires assessment against the Bushfire Hazard Overlay Code. The purpose of the Bushfire hazard overlay code is to minimise the threat of bushfire to people and property.

Response

The proposed development is designed to maintain the safety of people and property with respect to bushfire hazard. A detailed assessment against the code is included in **Appendix D**.

5.5.3 Transport Infrastructure Overlay Code

Overlay mapping identifies that the proposal requires assessment against the Transport Infrastructure Overlay Code. The purpose of the Transport Infrastructure Overlay Code is to promote the ongoing and expanded use of rail corridors within the shire for the transportation of passengers and freight.

Response

The subject site is not within proximity to an active or inactive railway corridor and will not impact upon rail freight networks. Detailed assessment against the code is not necessary for this proposal.



5.6 DEVELOPMENT CODES

5.6.1 Accommodation Activities Code

In accordance with the *Mareeba Shire Council Planning Scheme 2016* table of assessment, the development requires assessment against the Accommodation Activities Code. The purpose of the Accommodation Activities Code is to facilitate the provision of Accommodation activities in appropriate locations throughout the shire.

Response

A detailed assessment against the code is included in **Appendix D**.

5.6.2 Landscaping Code

In accordance with the Mareeba Shire Council Planning Scheme 2016 table of assessment, the development requires assessment against the Landscaping Code. The purpose of the Landscaping Code is to ensure all development is landscaped to a standard that:

- (a) complements the scale and appearance of the development;
- (b) protects and enhances the amenity and environmental values of the site;
- (c) complements and enhances the streetscape and local landscape character; and
- (d) ensures effective buffering of incompatible land uses to protect local amenity.

Response

Each proposed lot intended for dual occupancy development can provide a minimum of 10% landscaping. All future landscaping can be conditioned in accordance with this code. Street landscaping will be addressed as part of future operational works applications. Detailed assessment against this code is not considered necessary.

5.6.3 Parking and Access Code

In accordance with the *Mareeba Shire Council Planning Scheme 2016* table of assessment, the development requires assessment against the Parking and Access Code. The purpose of the Parking and access code is to ensure:

- (a) parking areas are appropriately designed, constructed and maintained;
- (b) the efficient functioning of the development and the local road network; and
- (c) all development provides sufficient parking, loading/service and manoeuvring areas to meet the demand generated by the use.

Response

All lots are afforded safe access to a gazetted road, and access handles within proposed Lots 18, 22, and 35 can be established in accordance with this code. Crossovers for each lot will be provided at the time of construction of a dwelling.

A detailed assessment against the code is included in **Appendix D**.

5.6.4 Reconfiguring a Lot Code

In accordance with the *Mareeba Shire Council Planning Scheme 2016* table of assessment, the development requires assessment against the Reconfiguring a Lot Code. The purpose of the Reconfiguring a lot code is to ensure that land is:

- (a) arranged in a manner which is consistent with the intended scale and intensity of development within the area;
- (b) provided with access to appropriate movement and open space networks; and
- (c) contributes to housing diversity and accommodates a range of land uses.



Response

The proposal is consistent with the intent of the zone and is responsive to existing site constraints. The use of cul-de-sacs in this instance is necessary to ensure efficient use of the site. All lots are designed to maintain the amenity and safety of the area and adjoining uses.

Detailed assessment against this code is included in **Appendix D**.

5.6.5 Works, Services, and Infrastructure Code

In accordance with the *Mareeba Shire Council Planning Scheme 2016* table of assessment, the development requires assessment against the Works, Services, and Infrastructure Code. The purpose of the Works, Services and Infrastructure Code is to ensure that all development is appropriately serviced by physical infrastructure, public utilities and services and that work associated with development is carried out in a manner that does not adversely impact on the surrounding area.

Response

The proposal can be efficiently connected to the required public utility services and networks and development does not adversely impact upon existing drainage systems as demonstrated in the engineering report included in **Appendix E**.

Detailed assessment against this code is included in **Appendix D**.



6.0 CONCLUSION

This proposal has detailed a development application to Mareeba Shire Council seeking a Development Permit for Reconfiguring a Lot Reconfiguring a Lot (1 into 44 Lots, New Road, and Access and Services Easements) and Material Change of Use for Dual Occupancies on land at 2-18 Haren Street, Mareeba, formally described as Lot 1 on SP298397.

The abovementioned has demonstrated that the proposal aligns with the relevant provisions of the *Mareeba Shire Council Planning Scheme 2016*. Approval of the development will directly assist in meeting the growing housing demand within Mareeba.

The following conclusions can be drawn from the above referenced planning aspects of the proposal:-

- Development is conveniently located and provides for a variety of residential allotments consistent with the Low Density Residential Zone, unlocking critical housing for Mareeba;
- The proposal aligns with the future land use intent for the zone and precinct without impact to the existing character and amenity of the area;
- The proposed dual occupancy designs are appropriate for the site and local context;
- The proposal provides efficient connections to the appropriate utility services and infrastructure networks without impact to surrounding development; and
- The proposal maintains the safety of existing and future residents with regard to access and liveability.

Overall, it is considered that the proposed development is an appropriate response to the economic climate, subject land, surrounding uses and established character of Mareeba Shire. Subject to the imposition of relevant and reasonable conditions, we recommend Council approve the proposed development at Haren Street, Mareeba.

APPENDIX A

brazier motti



DA Form 1 – Development application details

Approved form (version 1.6 effective 2 August 2024) made under section 282 of the Planning Act 2016.

This form **must** be used to make a development application **involving code assessment or impact assessment**, except when applying for development involving only building work.

For a development application involving **building work only**, use *DA Form 2 – Building work details*.

For a development application involving **building work associated with any other type of assessable development (i.e. material change of use, operational work or reconfiguring a lot)**, use this form (*DA Form 1*) and parts 4 to 6 of *DA Form 2 – Building work details*.

Unless stated otherwise, all parts of this form **must** be completed in full and all required supporting information **must** accompany the development application.

One or more additional pages may be attached as a schedule to this development application if there is insufficient space on the form to include all the necessary information.

This form and any other form relevant to the development application must be used to make a development application relating to strategic port land and Brisbane core port land under the *Transport Infrastructure Act 1994*, and airport land under the *Airport Assets (Restructuring and Disposal) Act 2008*. For the purpose of assessing a development application relating to strategic port land and Brisbane core port land, any reference to a planning scheme is taken to mean a land use plan for the strategic port land, Brisbane port land use plan for Brisbane core port land, or a land use plan for airport land.

Note: All terms used in this form have the meaning given under the Planning Act 2016, the Planning Regulation 2017, or the Development Assessment Rules (DA Rules).

PART 1 – APPLICANT DETAILS

1) Applicant details	
Applicant name(s) (individual or company full name)	FS Developments Pty Ltd
Contact name (only applicable for companies)	
Postal address (P.O. Box or street address)	c/- Brazier Motti Pty Ltd, PO Box 1185
Suburb	CAIRNS
State	QLD
Postcode	4870
Country	AUSTRALIA
Contact number	(07) 4054 0400
Email address (non-mandatory)	cns.planning@braziermotti.com.au
Mobile number (non-mandatory)	
Fax number (non-mandatory)	
Applicant's reference number(s) (if applicable)	34692-004-01
1.1) Home-based business	
☐ Personal details to remain private in accordance with section 264(6) of <i>Planning Act 2016</i>	

2) Owner's consent	
2.1) Is written consent of the owner required for this development application?	
☒ Yes – the written consent of the owner(s) is attached to this development application	
☐ No – proceed to 3)	

PART 2 – LOCATION DETAILS

3) Location of the premises (complete 3.1) or 3.2), and 3.3) as applicable)

Note: Provide details below and attach a site plan for any or all premises part of the development application. For further information, see [DA Forms Guide: Relevant plans](#).

3.1) Street address and lot on plan

- ☒ Street address **AND** lot on plan (all lots must be listed), **or**
☐ Street address **AND** lot on plan for an adjoining or adjacent property of the premises (appropriate for development in water but adjoining or adjacent to land e.g. jetty, pontoon. All lots must be listed).

a)	Unit No.	Street No.	Street Name and Type	Suburb
		2-18	Haren Street	Mareeba
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)
	4880	1	SP298397	Mareeba Shire Council
b)	Unit No.	Street No.	Street Name and Type	Suburb
	Postcode	Lot No.	Plan Type and Number (e.g. RP, SP)	Local Government Area(s)

3.2) Coordinates of premises (appropriate for development in remote areas, over part of a lot or in water not adjoining or adjacent to land e.g. channel dredging in Moreton Bay)

Note: Place each set of coordinates in a separate row.

- ☐ Coordinates of premises by longitude and latitude

Longitude(s)	Latitude(s)	Datum	Local Government Area(s) (if applicable)
		☐ WGS84 ☐ GDA94 ☐ Other:	

- ☐ Coordinates of premises by easting and northing

Easting(s)	Northing(s)	Zone Ref.	Datum	Local Government Area(s) (if applicable)
		☐ 54 ☐ 55 ☐ 56	☐ WGS84 ☐ GDA94 ☐ Other:	

3.3) Additional premises

- ☐ Additional premises are relevant to this development application and the details of these premises have been attached in a schedule to this development application
☒ Not required

4) Identify any of the following that apply to the premises and provide any relevant details

- ☐ In or adjacent to a water body or watercourse or in or above an aquifer

Name of water body, watercourse or aquifer:

- ☐ On strategic port land under the *Transport Infrastructure Act 1994*

Lot on plan description of strategic port land:

Name of port authority for the lot:

- ☐ In a tidal area

Name of local government for the tidal area (if applicable):

Name of port authority for tidal area (if applicable)

☐ On airport land under the <i>Airport Assets (Restructuring and Disposal) Act 2008</i>
Name of airport:
☐ Listed on the Environmental Management Register (EMR) under the <i>Environmental Protection Act 1994</i>
EMR site identification:
☐ Listed on the Contaminated Land Register (CLR) under the <i>Environmental Protection Act 1994</i>
CLR site identification:

5) Are there any existing easements over the premises?

Note: Easement uses vary throughout Queensland and are to be identified correctly and accurately. For further information on easements and how they may affect the proposed development, see [DA Forms Guide](#).

- ☒ Yes – All easement locations, types and dimensions are included in plans submitted with this development application
- ☐ No

PART 3 – DEVELOPMENT DETAILS

Section 1 – Aspects of development

6.1) Provide details about the first development aspect

a) What is the type of development? *(tick only one box)*

- ☐ Material change of use ☒ Reconfiguring a lot ☐ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☒ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☒ Code assessment ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal (e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):

Reconfiguring 1 into 44 lots, new road, access and services easements.

e) Relevant plans

Note: *Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms guide: Relevant plans](#).*

- ☒ Relevant plans of the proposed development are attached to the development application

6.2) Provide details about the second development aspect

a) What is the type of development? *(tick only one box)*

- ☒ Material change of use ☐ Reconfiguring a lot ☐ Operational work ☐ Building work

b) What is the approval type? *(tick only one box)*

- ☒ Development permit ☐ Preliminary approval ☐ Preliminary approval that includes a variation approval

c) What is the level of assessment?

- ☒ Code assessment ☐ Impact assessment *(requires public notification)*

d) Provide a brief description of the proposal (e.g. 6 unit apartment building defined as multi-unit dwelling, reconfiguration of 1 lot into 3 lots):

9 Dual Occupancies

e) Relevant plans

Note: *Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).*

- ☒ Relevant plans of the proposed development are attached to the development application

6.3) Additional aspects of development

- ☐ Additional aspects of development are relevant to this development application and the details for these aspects that would be required under Part 3 Section 1 of this form have been attached to this development application
- ☒ Not required

6.4) Is the application for State facilitated development?

- ☐ Yes - Has a notice of declaration been given by the Minister?
- ☒ No

Section 2 – Further development details**7) Does the proposed development application involve any of the following?**

Material change of use	☒ Yes – complete division 1 if assessable against a local planning instrument
Reconfiguring a lot	☒ Yes – complete division 2
Operational work	☐ Yes – complete division 3
Building work	☐ Yes – complete <i>DA Form 2 – Building work details</i>

Division 1 – Material change of use

Note: This division is only required to be completed if any part of the development application involves a material change of use assessable against a local planning instrument.

8.1) Describe the proposed material change of use

Provide a general description of the proposed use	Provide the planning scheme definition <i>(include each definition in a new row)</i>	Number of dwelling units <i>(if applicable)</i>	Gross floor area (m ²) <i>(if applicable)</i>
Dual occupancy	Dual occupancy	18 (9 dual occupancy dwellings)	-

8.2) Does the proposed use involve the use of existing buildings on the premises?

- ☐ Yes
- ☒ No

8.3) Does the proposed development relate to temporary accepted development under the Planning Regulation?

- ☐ Yes – provide details below or include details in a schedule to this development application
- ☒ No

Provide a general description of the temporary accepted development	Specify the stated period dates under the Planning Regulation

Division 2 – Reconfiguring a lot

Note: This division is only required to be completed if any part of the development application involves reconfiguring a lot.

9.1) What is the total number of existing lots making up the premises?

One (1)

9.2) What is the nature of the lot reconfiguration? *(tick all applicable boxes)*

☒ Subdivision <i>(complete 10)</i>	☐ Dividing land into parts by agreement <i>(complete 11)</i>
☐ Boundary realignment <i>(complete 12)</i>	☒ Creating or changing an easement giving access to a lot from a constructed road <i>(complete 13)</i>

10) Subdivision				
10.1) For this development, how many lots are being created and what is the intended use of those lots:				
Intended use of lots created	Residential	Commercial	Industrial	Other, please specify:
Number of lots created	44			

10.2) Will the subdivision be staged?	
☐ Yes – provide additional details below ☒ No	
How many stages will the works include?	
What stage(s) will this development application apply to?	

11) Dividing land into parts by agreement – how many parts are being created and what is the intended use of the parts?				
Intended use of parts created	Residential	Commercial	Industrial	Other, please specify:
Number of parts created				

12) Boundary realignment			
12.1) What are the current and proposed areas for each lot comprising the premises?			
Current lot		Proposed lot	
Lot on plan description	Area (m ²)	Lot on plan description	Area (m ²)
12.2) What is the reason for the boundary realignment?			

13) What are the dimensions and nature of any existing easements being changed and/or any proposed easement? (attach schedule if there are more than two easements)				
Existing or proposed?	Width (m)	Length (m)	Purpose of the easement? (e.g. pedestrian access)	Identify the land/lot(s) benefitted by the easement
Proposed Emt within Proposed Lot 18	5.0	20.3	Access	Proposed Lot 17
Proposed Emt within Proposed Lot 22	7.0	45.0	Access	Proposed Lots 21 & 23
Proposed Emt within Proposed Lot 33	3.0	38.1	Drainage	In gross to Council

Division 3 – Operational work

Note: This division is only required to be completed if any part of the development application involves operational work.

14.1) What is the nature of the operational work?	
☐ Road work ☐ Drainage work ☐ Landscaping ☐ Other – please specify:	☐ Stormwater ☐ Earthworks ☐ Signage ☐ Water infrastructure ☐ Sewage infrastructure ☐ Clearing vegetation
14.2) Is the operational work necessary to facilitate the creation of new lots? (e.g. subdivision)	
☐ Yes – specify number of new lots:	
☐ No	

14.3) What is the monetary value of the proposed operational work? (include GST, materials and labour)

\$

PART 4 – ASSESSMENT MANAGER DETAILS

15) Identify the assessment manager(s) who will be assessing this development application

Mareeba Shire Council

16) Has the local government agreed to apply a superseded planning scheme for this development application?

- ☐ Yes – a copy of the decision notice is attached to this development application
- ☐ The local government is taken to have agreed to the superseded planning scheme request – relevant documents attached
- ☒ No

PART 5 – REFERRAL DETAILS

17) Does this development application include any aspects that have any referral requirements?

Note: A development application will require referral if prescribed by the Planning Regulation 2017.

- ☐ No, there are no referral requirements relevant to any development aspects identified in this development application – proceed to Part 6

Matters requiring referral to the **Chief Executive of the Planning Act 2016:**

- ☐ Clearing native vegetation
- ☐ Contaminated land (*unexploded ordnance*)
- ☐ Environmentally relevant activities (ERA) (*only if the ERA has not been devolved to a local government*)
- ☐ Fisheries – aquaculture
- ☐ Fisheries – declared fish habitat area
- ☐ Fisheries – marine plants
- ☐ Fisheries – waterway barrier works
- ☐ Hazardous chemical facilities
- ☐ Heritage places – Queensland heritage place (*on or near a Queensland heritage place*)
- ☐ Infrastructure-related referrals – designated premises
- ☒ Infrastructure-related referrals – state transport infrastructure
- ☒ Infrastructure-related referrals – State transport corridor and future State transport corridor
- ☐ Infrastructure-related referrals – State-controlled transport tunnels and future state-controlled transport tunnels
- ☐ Infrastructure-related referrals – near a state-controlled road intersection
- ☐ Koala habitat in SEQ region – interfering with koala habitat in koala habitat areas outside koala priority areas
- ☐ Koala habitat in SEQ region – key resource areas
- ☐ Ports – Brisbane core port land – near a State transport corridor or future State transport corridor
- ☐ Ports – Brisbane core port land – environmentally relevant activity (ERA)
- ☐ Ports – Brisbane core port land – tidal works or work in a coastal management district
- ☐ Ports – Brisbane core port land – hazardous chemical facility
- ☐ Ports – Brisbane core port land – taking or interfering with water
- ☐ Ports – Brisbane core port land – referable dams
- ☐ Ports – Brisbane core port land – fisheries
- ☐ Ports – Land within Port of Brisbane's port limits (*below high-water mark*)
- ☐ SEQ development area
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – tourist activity or sport and recreation activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – community activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – indoor recreation
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – urban activity
- ☐ SEQ regional landscape and rural production area or SEQ rural living area – combined use
- ☐ SEQ northern inter-urban break – tourist activity or sport and recreation activity



Queensland
Government

- ☐ SEQ northern inter-urban break – community activity
- ☐ SEQ northern inter-urban break – indoor recreation
- ☐ SEQ northern inter-urban break – urban activity
- ☐ SEQ northern inter-urban break – combined use
- ☐ Tidal works or works in a coastal management district
- ☐ Reconfiguring a lot in a coastal management district or for a canal
- ☐ Erosion prone area in a coastal management district
- ☐ Urban design
- ☐ Water-related development – taking or interfering with water
- ☐ Water-related development – removing quarry material *(from a watercourse or lake)*
- ☐ Water-related development – referable dams
- ☐ Water-related development – levees *(category 3 levees only)*
- ☐ Wetland protection area

Matters requiring referral to the **local government**:

- ☐ Airport land
- ☐ Environmentally relevant activities (ERA) *(only if the ERA has been devolved to local government)*
- ☐ Heritage places – Local heritage places

Matters requiring referral to the **Chief Executive of the distribution entity or transmission entity**:

- ☐ Infrastructure-related referrals – Electricity infrastructure

Matters requiring referral to:

- The **Chief Executive of the holder of the licence**, if not an individual
- The **holder of the licence**, if the holder of the licence is an individual
- ☐ Infrastructure-related referrals – Oil and gas infrastructure

Matters requiring referral to the **Brisbane City Council**:

- ☐ Ports – Brisbane core port land

Matters requiring referral to the **Minister responsible for administering the Transport Infrastructure Act 1994**:

- ☐ Ports – Brisbane core port land *(where inconsistent with the Brisbane port LUP for transport reasons)*
- ☐ Ports – Strategic port land

Matters requiring referral to the **relevant port operator**, if applicant is not port operator:

- ☐ Ports – Land within Port of Brisbane's port limits *(below high-water mark)*

Matters requiring referral to the **Chief Executive of the relevant port authority**:

- ☐ Ports – Land within limits of another port *(below high-water mark)*

Matters requiring referral to the **Gold Coast Waterways Authority**:

- ☐ Tidal works or work in a coastal management district *(in Gold Coast waters)*

Matters requiring referral to the **Queensland Fire and Emergency Service**:

- ☐ Tidal works or work in a coastal management district *(involving a marina (more than six vessel berths))*

18) Has any referral agency provided a referral response for this development application?

- ☐ Yes – referral response(s) received and listed below are attached to this development application
- ☒ No

Referral requirement	Referral agency	Date of referral response

Identify and describe any changes made to the proposed development application that was the subject of the referral response and this development application, or include details in a schedule to this development application *(if applicable)*.

PART 6 – INFORMATION REQUEST

19) Information request under the DA Rules

☒ I agree to receive an information request if determined necessary for this development application

☐ I do not agree to accept an information request for this development application

Note: By not agreeing to accept an information request I, the applicant, acknowledge:

- that this development application will be assessed and decided based on the information provided when making this development application and the assessment manager and any referral agencies relevant to the development application are not obligated under the DA Rules to accept any additional information provided by the applicant for the development application unless agreed to by the relevant parties
- Part 3 under Chapter 1 of the DA Rules will still apply if the application is an application listed under section 11.3 of the DA Rules or
- Part 2 under Chapter 2 of the DA Rules will still apply if the application is for state facilitated development

Further advice about information requests is contained in the [DA Forms Guide](#).

PART 7 – FURTHER DETAILS

20) Are there any associated development applications or current approvals? (e.g. a preliminary approval)

☐ Yes – provide details below or include details in a schedule to this development application

☒ No

List of approval/development application references	Reference number	Date	Assessment manager
☐ Approval			
☐ Development application			
☐ Approval			
☐ Development application			

21) Has the portable long service leave levy been paid? (only applicable to development applications involving building work or operational work)

☐ Yes – a copy of the receipted QLeave form is attached to this development application

☐ No – I, the applicant will provide evidence that the portable long service leave levy has been paid before the assessment manager decides the development application. I acknowledge that the assessment manager may give a development approval only if I provide evidence that the portable long service leave levy has been paid

☒ Not applicable (e.g. building and construction work is less than \$150,000 excluding GST)

Amount paid	Date paid (dd/mm/yy)	QLeave levy number (A, B or E)
\$		

22) Is this development application in response to a show cause notice or required as a result of an enforcement notice?

☐ Yes – show cause or enforcement notice is attached

☒ No

23) Further legislative requirements

Environmentally relevant activities

23.1) Is this development application also taken to be an application for an environmental authority for an **Environmentally Relevant Activity (ERA)** under section 115 of the *Environmental Protection Act 1994*?

- ☐ Yes – the required attachment (form ESR/2015/1791) for an application for an environmental authority accompanies this development application, and details are provided in the table below
- ☒ No

Note: Application for an environmental authority can be found by searching “ESR/2015/1791” as a search term at www.qld.gov.au. An ERA requires an environmental authority to operate. See www.business.qld.gov.au for further information.

Proposed ERA number:		Proposed ERA threshold:	
Proposed ERA name:			

- ☐ Multiple ERAs are applicable to this development application and the details have been attached in a schedule to this development application.

Hazardous chemical facilities

23.2) Is this development application for a **hazardous chemical facility**?

- ☐ Yes – *Form 536: Notification of a facility exceeding 10% of schedule 15 threshold* is attached to this development application
- ☒ No

Note: See www.business.qld.gov.au for further information about hazardous chemical notifications.

Clearing native vegetation

23.3) Does this development application involve **clearing native vegetation** that requires written confirmation that the chief executive of the *Vegetation Management Act 1999* is satisfied the clearing is for a relevant purpose under section 22A of the *Vegetation Management Act 1999*?

- ☐ Yes – this development application includes written confirmation from the chief executive of the *Vegetation Management Act 1999* (s22A determination)
- ☒ No

Note: 1. Where a development application for operational work or material change of use requires a s22A determination and this is not included, the development application is prohibited development.
2. See <https://www.qld.gov.au/environment/land/vegetation/applying> for further information on how to obtain a s22A determination.

Environmental offsets

23.4) Is this development application taken to be a prescribed activity that may have a significant residual impact on a **prescribed environmental matter** under the *Environmental Offsets Act 2014*?

- ☐ Yes – I acknowledge that an environmental offset must be provided for any prescribed activity assessed as having a significant residual impact on a prescribed environmental matter
- ☒ No

Note: The environmental offset section of the Queensland Government's website can be accessed at www.qld.gov.au for further information on environmental offsets.

Koala habitat in SEQ Region

23.5) Does this development application involve a material change of use, reconfiguring a lot or operational work which is assessable development under Schedule 10, Part 10 of the Planning Regulation 2017?

- ☐ Yes – the development application involves premises in the koala habitat area in the koala priority area
- ☐ Yes – the development application involves premises in the koala habitat area outside the koala priority area
- ☒ No

Note: If a koala habitat area determination has been obtained for this premises and is current over the land, it should be provided as part of this development application. See koala habitat area guidance materials at www.desi.qld.gov.au for further information.

Water resources

23.6) Does this development application involve **taking or interfering with underground water through an artesian or subartesian bore, taking or interfering with water in a watercourse, lake or spring, or taking overland flow water under the *Water Act 2000***?

- ☐ Yes – the relevant template is completed and attached to this development application and I acknowledge that a relevant authorisation or licence under the *Water Act 2000* may be required prior to commencing development
- ☒ No

Note: Contact the Department of Resources at www.resources.qld.gov.au for further information.

DA templates are available from planning.statedevelopment.qld.gov.au. If the development application involves:

- Taking or interfering with underground water through an artesian or subartesian bore: complete DA Form 1 Template 1
- Taking or interfering with water in a watercourse, lake or spring: complete DA Form 1 Template 2
- Taking overland flow water: complete DA Form 1 Template 3.

Waterway barrier works

23.7) Does this application involve **waterway barrier works**?

- ☐ Yes – the relevant template is completed and attached to this development application
- ☒ No

DA templates are available from planning.statedevelopment.qld.gov.au. For a development application involving waterway barrier works, complete DA Form 1 Template 4.

Marine activities

23.8) Does this development application involve **aquaculture, works within a declared fish habitat area or removal, disturbance or destruction of marine plants**?

- ☐ Yes – an associated resource allocation authority is attached to this development application, if required under the *Fisheries Act 1994*
- ☒ No

Note: See guidance materials at www.daf.qld.gov.au for further information.

Quarry materials from a watercourse or lake

23.9) Does this development application involve the **removal of quarry materials from a watercourse or lake under the *Water Act 2000***?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
- ☒ No

Note: Contact the Department of Resources at www.resources.qld.gov.au and www.business.qld.gov.au for further information.

Quarry materials from land under tidal waters

23.10) Does this development application involve the **removal of quarry materials from land under tidal water under the *Coastal Protection and Management Act 1995***?

- ☐ Yes – I acknowledge that a quarry material allocation notice must be obtained prior to commencing development
- ☒ No

Note: Contact the Department of Environment, Science and Innovation at www.desi.qld.gov.au for further information.

Referable dams

23.11) Does this development application involve a **referable dam** required to be failure impact assessed under section 343 of the *Water Supply (Safety and Reliability) Act 2008* (the *Water Supply Act*)?

- ☐ Yes – the 'Notice Accepting a Failure Impact Assessment' from the chief executive administering the *Water Supply Act* is attached to this development application
- ☒ No

Note: See guidance materials at www.resources.qld.gov.au for further information.

Tidal work or development within a coastal management district

23.12) Does this development application involve **tidal work or development in a coastal management district**?

- ☐ Yes – the following is included with this development application:
- ☐ Evidence the proposal meets the code for assessable development that is prescribed tidal work (*only required if application involves prescribed tidal work*)
 - ☐ A certificate of title

☒ No

Note: See guidance materials at www.desi.qld.gov.au for further information.

Queensland and local heritage places

23.13) Does this development application propose development on or adjoining a place entered in the **Queensland heritage register** or on a place entered in a local government's **Local Heritage Register**?

☐ Yes – details of the heritage place are provided in the table below

☒ No

Note: See guidance materials at www.desi.qld.gov.au for information requirements regarding development of Queensland heritage places.

For a heritage place that has cultural heritage significance as a local heritage place and a Queensland heritage place, provisions are in place under the Planning Act 2016 that limit a local categorising instrument from including an assessment benchmark about the effect or impact of, development on the stated cultural heritage significance of that place. See guidance materials at www.planning.statedevelopment.qld.gov.au for information regarding assessment of Queensland heritage places.

Name of the heritage place:

Place ID:

Decision under section 62 of the Transport Infrastructure Act 1994

23.14) Does this development application involve new or changed access to a state-controlled road?

☐ Yes – this application will be taken to be an application for a decision under section 62 of the *Transport Infrastructure Act 1994* (subject to the conditions in section 75 of the *Transport Infrastructure Act 1994* being satisfied)

☒ No

Walkable neighbourhoods assessment benchmarks under Schedule 12A of the Planning Regulation

23.15) Does this development application involve reconfiguring a lot into 2 or more lots in certain residential zones (except rural residential zones), where at least one road is created or extended?

☒ Yes – Schedule 12A is applicable to the development application and the assessment benchmarks contained in schedule 12A have been considered

☐ No

Note: See guidance materials at www.planning.statedevelopment.qld.gov.au for further information.

PART 8 – CHECKLIST AND APPLICANT DECLARATION

24) Development application checklist

I have identified the assessment manager in question 15 and all relevant referral requirement(s) in question 17

☒ Yes

Note: See the *Planning Regulation 2017* for referral requirements

If building work is associated with the proposed development, Parts 4 to 6 of [DA Form 2 – Building work details](#) have been completed and attached to this development application

☐ Yes

☒ Not applicable

Supporting information addressing any applicable assessment benchmarks is with the development application

Note: This is a mandatory requirement and includes any relevant templates under question 23, a planning report and any technical reports required by the relevant categorising instruments (e.g. local government planning schemes, State Planning Policy, State Development Assessment Provisions). For further information, see [DA Forms Guide: Planning Report Template](#).

☒ Yes

Relevant plans of the development are attached to this development application

Note: Relevant plans are required to be submitted for all aspects of this development application. For further information, see [DA Forms Guide: Relevant plans](#).

☒ Yes

The portable long service leave levy for QLeave has been paid, or will be paid before a development permit is issued (see 21)

☐ Yes

☒ Not applicable

25) Applicant declaration

- ☒ By making this development application, I declare that all information in this development application is true and correct
- ☒ Where an email address is provided in Part 1 of this form, I consent to receive future electronic communications from the assessment manager and any referral agency for the development application where written information is required or permitted pursuant to sections 11 and 12 of the *Electronic Transactions Act 2001*

Note: It is unlawful to intentionally provide false or misleading information.

Privacy – Personal information collected in this form will be used by the assessment manager and/or chosen assessment manager, any relevant referral agency and/or building certifier (including any professional advisers which may be engaged by those entities) while processing, assessing and deciding the development application. All information relating to this development application may be available for inspection and purchase, and/or published on the assessment manager's and/or referral agency's website.

Personal information will not be disclosed for a purpose unrelated to the *Planning Act 2016*, Planning Regulation 2017 and the DA Rules except where:

- such disclosure is in accordance with the provisions about public access to documents contained in the *Planning Act 2016* and the Planning Regulation 2017, and the access rules made under the *Planning Act 2016* and Planning Regulation 2017; or
- required by other legislation (including the *Right to Information Act 2009*); or
- otherwise required by law.

This information may be stored in relevant databases. The information collected will be retained as required by the *Public Records Act 2002*.

PART 9 – FOR COMPLETION OF THE ASSESSMENT MANAGER – FOR OFFICE USE ONLY

Date received: Reference number(s):

Notification of engagement of alternative assessment manager

Prescribed assessment manager	
Name of chosen assessment manager	
Date chosen assessment manager engaged	
Contact number of chosen assessment manager	
Relevant licence number(s) of chosen assessment manager	

QLeave notification and payment

Note: For completion by assessment manager if applicable

Description of the work	
QLeave project number	
Amount paid (\$)	Date paid (dd/mm/yy)
Date receipted form sighted by assessment manager	
Name of officer who sighted the form	

Company owner's consent to the making of a development application under the Planning Act 2016

SIGNATURE CARE LAND HOLDINGS PTY LTD A.C.N. 600 935 760 TRUSTEE UNDER INSTRUMENT 719490913

as owner of the premises identified as follows:

Lot 1 on SP298397

consent to the making of a development application under the *Planning Act 2016* by:

FS Developments Pty Ltd
c/- Brazier Motti Pty Ltd

on the premises described above for:

Reconfiguring a Lot (1 Lot into 44 Lots, New Road, and Access and Services Easements) & Material Change of Use (Dual Occupancies)

Name: GRAEME CROFT

Position: Director

Date: 4 August 2026

Name: STEPHEN POLLOCK

Position: Director

Date: 4 August 2026

APPENDIX B

brazier motti



Queensland Titles Registry Pty Ltd
ABN 23 648 568 101

Title Reference:	51121296	Search Date:	21/07/2026 15:42
Date Title Created:	09/11/2017	Request No:	56965241
Previous Title:	50927068		

ESTATE AND LAND

Estate in Fee Simple

LOT 1 SURVEY PLAN 298397

Local Government: MAREEBA

REGISTERED OWNER

Dealing No: 719490913 01/07/2019

SIGNATURE CARE LAND HOLDINGS PTY LTD A.C.N. 600 935 760 TRUSTEE
UNDER INSTRUMENT 719490913

EASEMENTS, ENCUMBRANCES AND INTERESTS

1. Rights and interests reserved to the Crown by
Deed of Grant No. 40067134 (Lot 157 on CP NR5658)
2. EASEMENT IN GROSS No 718335766 17/10/2017 at 09:40
burdening the land
COUNCIL OF THE SHIRE OF MAREEBA
over
EASEMENT A ON SP298397

ADMINISTRATIVE ADVICES

NIL

UNREGISTERED DEALINGS

NIL

Caution - Charges do not necessarily appear in order of priority

** End of Current Title Search **



0		50 mm		100 mm		150 mm		State copyright reserved.	
<i>Plan of Lots 1 & 2 and Emt A in Lot 1</i> <i>Cancelling Lot 157 on NR5658</i> LOCAL <i>MAREEBA SHIRE</i> GOVERNMENT: <i>COUNCIL</i> LOCALITY: <i>MAREEBA</i> Meridian: <i>NR5658</i>						Scale: <i>1:1500</i>			
						Format: STANDARD			
						 SP298397			
						Survey Records: <i>No</i>			

718335741

\$579.00
17/10/2017 09:39

CS 400 NT

WARNING : Folded or Mutilated Plans will not be accepted.
Plans may be rolled.
Information may not be placed in the outer margins.

Sheet
2 of
3

5. Lodged by

David Anthony Solicitor
PO BOX 53
MAREEBA Q 4880
Ph- 4092 2600.
(Include address, phone number, reference, and Lodger Code)

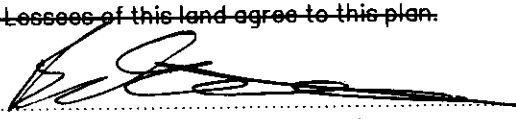
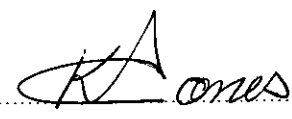
1. Certificate of Registered Owners or Lessees.

✓ We BRET ANDREW STEVENSON
KYLIE MAREE JONES TRUSTEE
UNDER INSTRUMENT 715978940

(Names in full)

* as Registered Owners of this land agree to this plan and dedicate the Public Use
Land as shown hereon in accordance with Section 50 of the Land Title Act 1994.

* as Lessees of this land agree to this plan.

 
Signature of *Registered Owners *Lessees

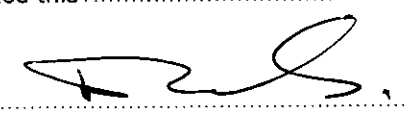
* Rule out whichever is inapplicable

2. Planning Body Approval.

* Mareeba Shire Council
hereby approves this plan in accordance with the:
%

Sustainable Planning Act 2009

Dated this 13th day of October 2017

 # Peter Franks
Chief Executive Officer
#

* Insert the name of the Planning Body.
Insert designation of signatory or delegation

% Insert applicable approving legislation.

3. Plans with Community Management Statement :

CMS Number :
Name :

4. References :

Dept File :
Local Govt :
Surveyor : PRI23862

6. Existing

Created

Title Reference	Description	New Lots	Road	Secondary Interests
50927068	Lot 157 on NR5658	1 & 2	—	Emt A

MORTGAGE ALLOCATIONS

Mortgage	Lots Fully Encumbered	Lots Partially Encumbered
715978941	1 & 2	—

Notification issued to the owner(s) of Lot 157 on NR5658 &
Antonio Drive on 23/08/2017, in accordance with s.18 of the
Survey and Mapping Infrastructure Regulation 2014.

9. Building Format Plans only.

I certify that :

- * As far as it is practical to determine, no part of the building shown on this plan encroaches onto adjoining lots or road;
- * Part of the building shown on this plan encroaches onto adjoining * lots and road

Cadastral Surveyor/Director * Date
* delete words not required

10. Lodgement Fees :

Survey Deposit \$
Lodgement \$
..... New Titles \$
Photocopy \$
Postage \$
TOTAL \$

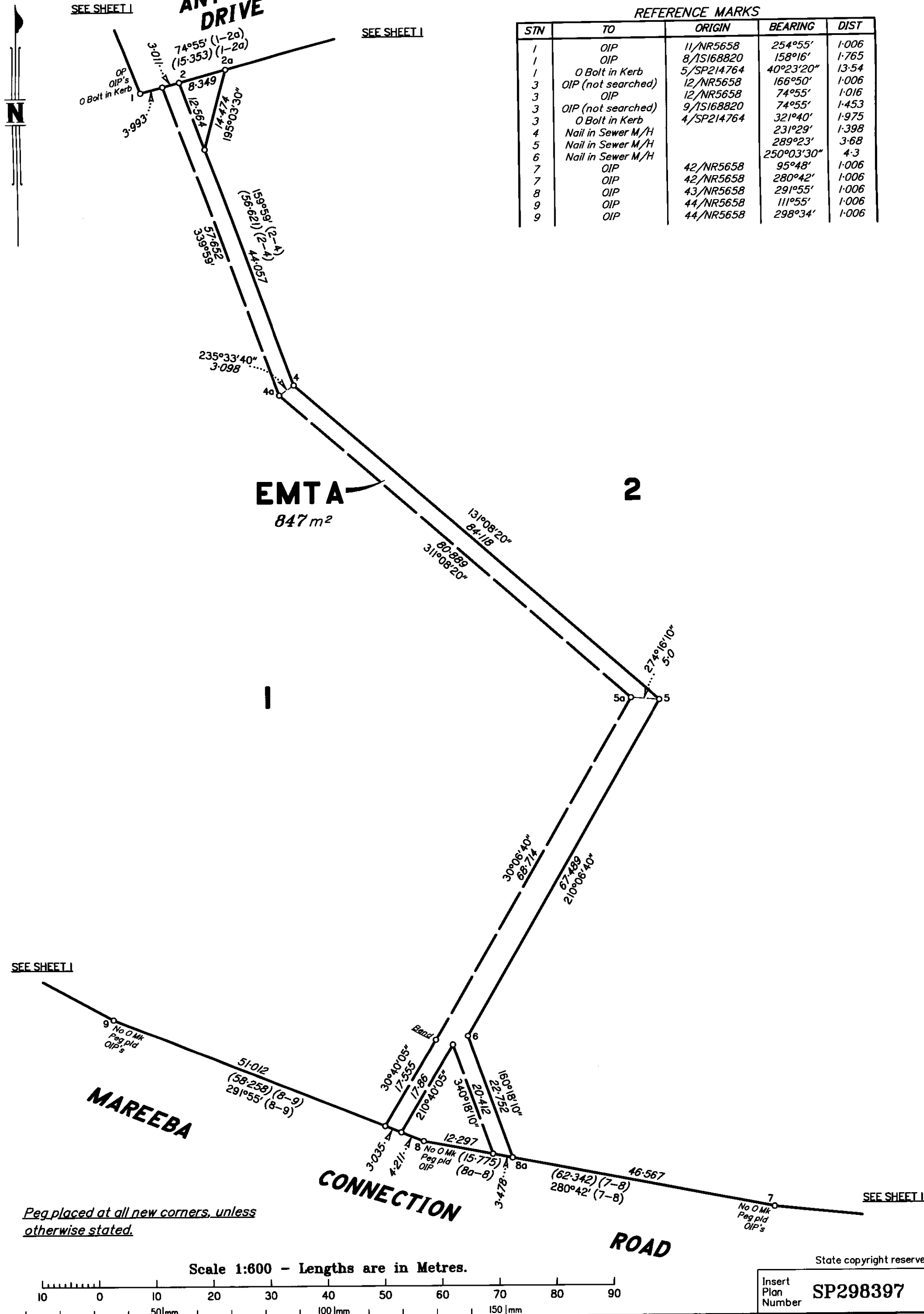
11. Insert
Plan
Number

SP298397

7. Orig Grant Allocation :

8. Passed & Endorsed :

By : RPS Australia East Pty Ltd
Date : 24/08/17
Signed : 
Designation : Cadastral Surveyor



APPENDIX C

brazier motti



State code 1: Development in a state-controlled road environment

State Development Assessment Provisions guideline - State Code 1: Development in a state-controlled road environment. This guideline provides direction on how to address State Code 1.

Table 1.1 Development in general

Performance outcomes	Acceptable outcomes	Response
Buildings, structures, infrastructure, services and utilities		
PO1 The location of the development does not create a safety hazard for users of the state-controlled road .	AO1.1 Development is not located in a state-controlled road . AND AO1.2 Development can be maintained without requiring access to a state-controlled road .	Complies AO1.1 and 1.2 Development is not within a state-controlled road and can be maintained without access to a state-controlled road. The road verge adjoining the subject land approximately 20m in width, hence the propose development is adequately separated from the SCR. Only a single dual occupancy is proposed within the 25m SCR buffer, within proposed Lot 1, construction and development will not impact the structural integrity of the SCR. Access is from the new road from Haren Street only.
PO2 The design and construction of the development does not adversely impact the structural integrity or physical condition of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies PO2 Refer comment AO1.1 and 1.2.
PO3 The location of the development does not obstruct road transport infrastructure or adversely impact the operating performance of the state-controlled road .	No acceptable outcome is prescribed.	Complies PO3 Refer comment AO1.1 and 1.2.
PO4 The location, placement, design and operation of advertising devices, visible from the state-controlled road , do not create a safety hazard for users of the state-controlled road .	No acceptable outcome is prescribed.	Not applicable No advertising devices are proposed.
PO5 The design and construction of buildings and structures does not create a safety hazard by	AO5.1 Facades of buildings and structures fronting the state-	Complies AO5.1 to 5.4

State Development Assessment Provisions v3.6

State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
distracting users of the state-controlled road .	controlled road are made of non-reflective materials. AND AO5.2 Facades of buildings and structures do not direct or reflect point light sources into the face of oncoming traffic on the state-controlled road. AND AO5.3 External lighting of buildings and structures is not directed into the face of oncoming traffic on the state-controlled road. AND AO5.4 External lighting of buildings and structures does not involve flashing or laser lights.	The proposed dual occupancy within proposed Lot 1 is for a residential use and the design complies with AO5.1 to 5.4. The residential development will be screened from the SCR by a 2m high soundproof fence.
PO6 Road, pedestrian and bikeway bridges over a state-controlled road are designed and constructed to prevent projectiles from being thrown onto the state-controlled road .	AO6.1 Road, pedestrian and bikeway bridges over the state-controlled road include throw protection screens in accordance with section 4.11 of the Design Criteria for Bridges and Other Structures Manual, Department of Transport and Main Roads, 2020.	Not applicable No bridges proposed.
Landscaping		
PO7 The location of landscaping does not create a safety hazard for users of the state-controlled road .	AO7.1 Landscaping is not located in a state-controlled road. AND AO7.2 Landscaping can be maintained without requiring access to a state-controlled road. AND AO7.3 Landscaping does not block or obscure the sight lines for vehicular access to a state-controlled road.	Complies AO7.1 The proposal is for the reconfiguration of land and dual occupancies. No landscaping will encroach or obstruct the SCR.

State Development Assessment Provisions v3.6

State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
Stormwater and overland flow		
PO8 Stormwater run-off or overland flow from the development site does not create or exacerbate a safety hazard for users of the state-controlled road .	No acceptable outcome is prescribed.	Complies PO8 The subject site falls away from the State-controlled road and stormwater is managed accordingly. All stormwater is diverted to stormwater infrastructure adjacent Antonio Drive, not creating a safety hazard to users of Mareeba Connection Road. Refer to the engineering report included in Appendix E .
PO9 Stormwater run-off or overland flow from the development site does not result in a material worsening of the operating performance of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies PO9 Refer comment PO8.
PO10 Stormwater run-off or overland flow from the development site does not adversely impact the structural integrity or physical condition of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies PO10 Refer comment PO8.
PO11 Development ensures that stormwater is lawfully discharged.	AO11.1 Development does not create any new points of discharge to a state-controlled road . AND AO11.2 Development does not concentrate flows to a state-controlled road . AND AO11.3 Stormwater run-off is discharged to a lawful point of discharge . AND AO11.4 Development does not worsen the condition of an existing lawful point of discharge to the state-controlled road .	Complies AO11.1 – AO11.4 Refer comment PO8.
Flooding		
PO12 Development does not result in a material worsening of	AO12.1 For all flood events up to 1% annual exceedance	Complies AO12.1-AO12.3

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
flooding impacts within a state-controlled road .	probability, development results in negligible impacts (within +/- 10mm) to existing flood levels within a state-controlled road. AND AO12.2 For all flood events up to 1% annual exceedance probability, development results in negligible impacts (up to a 10% increase) to existing peak velocities within a state-controlled road. AND AO12.3 For all flood events up to 1% annual exceedance probability, development results in negligible impacts (up to a 10% increase) to existing time of submergence of a state-controlled road.	The land falls away from the State-controlled road, hence the proposal does not have the ability to impact upon the flood regime of the state-controlled road. Future development within the associated lots will be assessed accordingly. Development within proposed Lot 1 does not drain to the SCR. Refer to the engineering report included in Appendix E .
Drainage Infrastructure		
PO13 Drainage infrastructure does not create a safety hazard for users in the state-controlled road .	AO13.1 Drainage infrastructure is wholly contained within the development site, except at the lawful point of discharge. AND AO13.2 Drainage infrastructure can be maintained without requiring access to a state-controlled road.	Complies AO13.3 A new drainage path is proposed to divert overland flow from the SCR around the subject site and will be lawfully discharged to existing flow paths at Antonio Drive. No new drainage pipes are located within the SCR. Refer to the engineering report included in Appendix E .
PO14 Drainage infrastructure associated with, or within, a state-controlled road is constructed, and designed to ensure the structural integrity and physical condition of existing drainage infrastructure and the surrounding drainage network.	No acceptable outcome is prescribed.	Complies PO14 A new drainage path is proposed to divert overland flow from the SCR around the subject site and will be lawfully discharged to existing flow paths at Antonio Drive. Refer to the engineering report included in Appendix E .

Table 1.2 Vehicular access, road layout and local roads

Performance outcomes	Acceptable outcomes	Response
Vehicular access to a state-controlled road or within 100 metres of a state-controlled road intersection		
PO15 The location, design and operation of a new or changed	No acceptable outcome is prescribed.	Not applicable

State Development Assessment Provisions v3.6

State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
access to a state-controlled road does not compromise the safety of users of the state-controlled road .		No new or changed access to Mareeba Connection Road or within 100m of an SCR intersection is proposed.
PO16 The location, design and operation of a new or changed access does not adversely impact the functional requirements of the state-controlled road .	No acceptable outcome is prescribed.	Not applicable
PO17 The location, design and operation of a new or changed access is consistent with the future intent of the state-controlled road .	No acceptable outcome is prescribed.	Not applicable
PO18 New or changed access is consistent with the access for the relevant limited access road policy : 1. LAR 1 where direct access is prohibited; or 2. LAR 2 where access may be permitted, subject to assessment.	No acceptable outcome is prescribed.	Not applicable
PO19 New or changed access to a local road within 100 metres of an intersection with a state-controlled road does not compromise the safety of users of the state-controlled road .	No acceptable outcome is prescribed.	Not applicable
PO20 New or changed access to a local road within 100 metres of an intersection with a state-controlled road does not adversely impact on the operating performance of the intersection.	No acceptable outcome is prescribed.	Not applicable
Public passenger transport and active transport		
PO21 Development does not compromise the safety of users of public passenger transport infrastructure, public passenger services and active transport infrastructure .	No acceptable outcome is prescribed.	Complies PO21 The subject site is located within the Low Density Residential Zone. The proposal does not have the ability to compromise public or active transport infrastructure. The proposal produces a residential density consistent with the Low Density Residential Zone and is anticipated development for this area.
PO22 Development maintains the ability for people to access public passenger transport infrastructure, public passenger	No acceptable outcome is prescribed.	Complies PO22 The subject site is located within the Low Density Residential Zone. The proposal does not have the

State Development Assessment Provisions v3.6

State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
services and active transport infrastructure.		ability to compromise public or active transport infrastructure. Access is not compromised.
PO23 Development does not adversely impact the operating performance of public passenger transport infrastructure, public passenger services and active transport infrastructure.	No acceptable outcome is prescribed.	Complies PO23 The proposal does not interfere with public passenger transport infrastructure, public passenger services and active transport infrastructure.
PO24 Development does not adversely impact the structural integrity or physical condition of public passenger transport infrastructure and active transport infrastructure.	No acceptable outcome is prescribed.	Complies PO24 The proposal does not interfere with public passenger transport infrastructure, public passenger services and active transport infrastructure.

Table 1.3 Network impacts

Performance outcomes	Acceptable outcomes	Response
PO25 Development does not compromise the safety of users of the state-controlled road network.	No acceptable outcome is prescribed.	Complies PO25 The proposal is does not direct traffic onto the SCR. The proposal produces residential development at a density expected for the site and does not have the ability to compromise the safety of users of Mareeba Connection Road.
PO26 Development ensures no net worsening of the operating performance of the state-controlled road network.	No acceptable outcome is prescribed.	Complies PO26 The proposal produces residential lots at a density expected for the site and does not have the ability to compromise the safety of users of the SCR.
PO27 Traffic movements are not directed onto a state-controlled road where they can be accommodated on the local road network.	No acceptable outcome is prescribed.	Complies PO27 The proposal does not direct traffic onto the SCR.
PO28 Development involving haulage exceeding 10,000 tonnes per year does not adversely impact the pavement of a state-controlled road.	No acceptable outcome is prescribed.	Not applicable The proposal is for residential development only.
PO29 Development does not impede delivery of planned upgrades of state-controlled roads.	No acceptable outcome is prescribed.	Complies PO29 No planned upgrades are identified within proximity to the site.

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
PO30 Development does not impede delivery of corridor improvements located entirely within the state-controlled road corridor .	No acceptable outcome is prescribed.	Complies PO30 The proposal does not impede into the state-controlled road corridor.

Table 1.4 Filling, excavation, building foundations and retaining structures

Performance outcomes	Acceptable outcomes	Response
PO31 Development does not create a safety hazard for users of the state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies PO31 Only minor earthworks may be required to establish the new drainage path within the SCR road verge. Refer to the engineering report included in Appendix E . No major earthworks are required, and works will not impact the safety of the SCR.
PO32 Development does not adversely impact the operating performance of the state-controlled road .	No acceptable outcome is prescribed.	Complies PO32 Refer comment PO31.
PO33 Development does not undermine, damage or cause subsidence of a state-controlled road .	No acceptable outcome is prescribed.	Complies PO33 Refer comment PO31.
PO34 Development does not cause ground water disturbance in a state-controlled road .	No acceptable outcome is prescribed.	Complies PO34 Refer comment PO31.
PO35 Excavation, boring, piling, blasting and fill compaction do not adversely impact the physical condition or structural integrity of a state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Complies PO35 Refer comment PO31.
PO36 Filling and excavation associated with the construction of new or changed access do not compromise the operation or capacity of existing drainage infrastructure for a state-controlled road .	No acceptable outcome is prescribed.	Not applicable No new access to SCR.

Table 1.5 Environmental emissions

Statutory note: Where a **state-controlled road** is co-located in the same transport corridor as a railway, the development should instead comply with Environmental emissions in State code 2: Development in a railway environment.

Performance outcomes	Acceptable outcomes	Response
Reconfiguring a lot		

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
Involving the creation of 5 or fewer new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor		
PO37 Development minimises free field noise intrusion from a state-controlled road .	AO37.1 Development provides a noise barrier or earth mound which is designed, sited and constructed: 1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.1); 2. in accordance with: a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. OR AO37.2 Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound. OR AO37.3 Development provides a solid gap-free fence or other solid gap-free structure along the full extent of the boundary closest to the state-controlled road.	Not applicable
Involving the creation of 6 or more new residential lots adjacent to a state-controlled road or type 1 multi-modal corridor		
PO38 Reconfiguring a lot minimises free field noise intrusion from a state-controlled road .	AO38.1 Development provides noise barrier or earth mound which is designed, sited and constructed: 1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.1); 2. in accordance with:	Complies AO38.1 Development provides a noise barrier in accordance with this outcome. The subject site will be screened with a 2m high sound proof fence to the SCR frontage. The subject site is further separated from the

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
	a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. OR AO38.2 Development achieves the maximum free field acoustic levels in reference table 2 (item 2.1) by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.	SCR by a significant 20m wide road verge, providing additional acoustic relief.
Material change of use (accommodation activity)		
Ground floor level requirements adjacent to a state-controlled road or type 1 multi-modal corridor		
PO39 Development minimises noise intrusion from a state-controlled road in private open space .	AO39.1 Development provides a noise barrier or earth mound which is designed, sited and constructed: 1. to achieve the maximum free field acoustic levels in reference table 2 (item 2.2) for private open space at the ground floor level; 2. in accordance with: a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020.	Complies AO39.1 Development provides a noise barrier in accordance with this outcome. The subject site will be screened with a 2m high sound proof fence to the SCR frontage. The subject site is further separated from the SCR by a significant 20m wide road verge, providing additional acoustic relief.

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
	OR AO39.2 Development achieves the maximum free field acoustic level in reference table 2 (item 2.2) for private open space by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.	
PO40 Development (excluding a relevant residential building or relocated building) minimises noise intrusion from a state-controlled road in habitable rooms at the facade.	AO40.1 Development (excluding a relevant residential building or relocated building) provides a noise barrier or earth mound which is designed, sited and constructed: 1. to achieve the maximum building façade acoustic level in reference table 1 (item 1.1) for habitable rooms; 2. in accordance with: a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020. OR AO40.2 Development (excluding a relevant residential building or relocated building) achieves the maximum building façade acoustic level in reference table 1 (item 1.1) for habitable rooms by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.	Not applicable
PO41 Habitable rooms (excluding a relevant residential building or relocated building) are designed and constructed using materials to achieve the	No acceptable outcome is provided.	Complies PO41 The dual occupancy design within proposed Lot 1 is appropriately designed to provide acoustic mitigation, in addition to

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
maximum internal acoustic level in reference table 3 (item 3.1).		the significant 20m separation of the road verge to the SCR.
Above ground floor level requirements (accommodation activity) adjacent to a state-controlled road or type 1 multi-modal corridor		
PO42 Balconies, podiums, and roof decks include: 1. a continuous solid gap-free structure or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia); 2. highly acoustically absorbent material treatment for the total area of the soffit above balconies, podiums, and roof decks.	No acceptable outcome is provided.	Not applicable
PO43 Habitable rooms (excluding a relevant residential building or relocated building) are designed and constructed using materials to achieve the maximum internal acoustic level in reference table 3 (item 3.1).	No acceptable outcome is provided.	Not applicable
Material change of use (other uses)		
Ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor		
PO44 Development: 1. provides a noise barrier or earth mound that is designed, sited and constructed: a. to achieve the maximum free field acoustic level in reference table 2 (item 2.3) for all outdoor education areas and outdoor play areas; b. in accordance with: i. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and	No acceptable outcome is provided.	Not applicable

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
ii. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; iii. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or 2. achieves the maximum free field acoustic level in reference table 2 (item 2.3) for all outdoor education areas and outdoor play areas by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.		
PO45 Development involving a childcare centre or educational establishment: 1. provides a noise barrier or earth mound that is designed, sited and constructed: 2. to achieve the maximum building facade acoustic level in reference table 1 (item 1.2); 3. in accordance with: a. Chapter 7 integrated noise barrier design of the Transport Noise Management Code of Practice: Volume 1 (Road Traffic Noise), Department of Transport and Main Roads, 2013; b. Technical Specification-MRTS15 Noise Fences, Transport and Main Roads, 2019; c. Technical Specification-MRTS04 General Earthworks, Transport and Main Roads, 2020; or	No acceptable outcome is provided.	Not applicable

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
4. achieves the maximum building facade acoustic level in reference table 1 (item 1.2) by alternative noise attenuation measures where it is not practical to provide a noise barrier or earth mound.		
PO46 Development involving: 1. indoor education areas and indoor play areas ; or 2. sleeping rooms in a childcare centre ; or 3. patient care areas in a hospital achieves the maximum internal acoustic level in reference table 3 (items 3.2-3.4).	No acceptable outcome is provided.	Not applicable
Above ground floor level requirements (childcare centre, educational establishment, hospital) adjacent to a state-controlled road or type 1 multi-modal corridor		
PO47 Development involving a childcare centre or educational establishment which have balconies, podiums or elevated outdoor play areas predicted to exceed the maximum free field acoustic level in reference table 2 (item 2.3) due to noise from a state-controlled road are provided with: 1. a continuous solid gap-free structure or balustrade (excluding gaps required for drainage purposes to comply with the Building Code of Australia); 2. highly acoustically absorbent material treatment for the total area of the soffit above balconies or elevated outdoor play areas .	No acceptable outcome is provided.	Not applicable
PO48 Development including: 1. indoor education areas and indoor play areas in a childcare centre or educational establishment ; or 2. sleeping rooms in a childcare centre ; or 3. patient care areas in a hospital located above ground level, is designed and constructed to achieve	No acceptable outcome is provided.	Not applicable

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
the maximum internal acoustic level in reference table 3 (items 3.2-3.4).		
Air, light and vibration		
PO49 Private open space, outdoor education areas and outdoor play areas are protected from air quality impacts from a state-controlled road .	AO49.1 Each dwelling or unit has access to a private open space which is shielded from a state-controlled road by a building, solid gap-free fence, or other solid gap-free structure. OR AO49.2 Each outdoor education area and outdoor play area is shielded from a state-controlled road by a building, solid gap-free fence, or other solid gap-free structure.	Not applicable
PO50 Patient care areas within hospitals are protected from vibration impacts from a state-controlled road or type 1 multi-modal corridor .	AO50.1 Hospitals are designed and constructed to ensure vibration in the patient treatment area does not exceed a vibration dose value of $0.1\text{m/s}^{1.75}$. AND AO50.2 Hospitals are designed and constructed to ensure vibration in the ward of a patient care area does not exceed a vibration dose value of $0.4\text{m/s}^{1.75}$.	
PO51 Development is designed and sited to ensure light from infrastructure within, and from users of, a state-controlled road or type 1 multi-modal corridor, does not: 1. intrude into buildings during night hours (10pm to 6am); 2. create unreasonable disturbance during evening hours (6pm to 10pm).	No acceptable outcomes are prescribed.	Not applicable

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State code 1: Development in a state-controlled road environment

Table 1.6: Development in a future state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
PO52 Development does not impede delivery of a future state-controlled road .	AO52.1 Development is not located in a future state-controlled road. OR ALL OF THE FOLLOWING APPLY: AO52.2 Development does not involve filling and excavation of, or material changes to, a future state-controlled road. AND AO52.3 The intensification of lots does not occur within a future state-controlled road. AND AO52.4 Development does not result in the landlocking of parcels once a future state-controlled road is delivered.	Not applicable The site is not within proximity to a future state-controlled road.
PO53 The location and design of new or changed access does not create a safety hazard for users of a future state-controlled road .	AO53.1 Development does not include new or changed access to a future state-controlled road .	Not applicable
PO54 Filling, excavation, building foundations and retaining structures do not undermine, damage or cause subsidence of a future state-controlled road .	No acceptable outcome is prescribed.	Not applicable
PO55 Development does not result in a material worsening of stormwater, flooding, overland flow or drainage impacts in a future state-controlled road or road transport infrastructure .	No acceptable outcome is prescribed.	Not applicable
PO56 Development ensures that stormwater is lawfully discharged.	AO56.1 Development does not create any new points of discharge to a future state-controlled road. AND	Not applicable

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State code 1: Development in a state-controlled road environment

Performance outcomes	Acceptable outcomes	Response
	AO56.2 Development does not concentrate flows to a future state-controlled road. AND AO56.3 Stormwater run-off is discharged to a lawful point of discharge. AND AO56.4 Development does not worsen the condition of an existing lawful point of discharge to the future state-controlled road.	

APPENDIX D

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Assessment of application against relevant Development Codes

6.2.6 Low density residential zone code

6.2.6.1 Application

- (1) This code applies to assessing development where:
 - (a) located in the Low density residential zone; and
 - (b) it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

6.2.6.2 Purpose

- (1) The purpose of the Low density residential zone code is to provide for predominantly dwelling houses supported by community uses and small-scale services and facilities that cater for local residents.
- (2) Mareeba Shire Council's purpose of the Low density residential zone code is to:
 - (a) maintain the integrity of established residential areas, which are characterised primarily by Dwelling houses and Dual occupancy development;
 - (b) provide opportunities for other forms of residential development where existing character and amenity will not be compromised; and
 - (c) facilitate non-residential development that directly supports the day to day needs of the immediate residential community, in new residential areas.
- (3) The purpose of the code will be achieved through the following overall outcomes:
 - (a) The dominant form of development is detached dwelling houses, on a range of lot sizes;
 - (b) In greenfield areas, in proximity to activity centres, a wider range of higher density residential development may occur where existing low density residential amenity is not compromised;
 - (c) High quality Residential care facilities and Retirement facilities are located on larger sites;
 - (d) Development provides for an efficient land use pattern and is well connected to other developments;
 - (e) Development is designed to provide safe and walkable neighbourhoods that connect residents to desirable destinations including schools, parks, shops and community facilities;
 - (f) Development facilitates other small-scale uses that integrate personal employment and residential activities, provided they complement local residential amenity;
 - (g) Development maintains a high level of residential amenity avoiding uses that introduce impacts associated with noise, hours of operation, traffic, advertising devices, visual amenity, privacy, lighting, odour and emissions;
 - (h) Development reflects and enhances the existing low density scale and character of the area;
 - (i) Development is supported by necessary transport infrastructure which is designed to provide and promote safe and efficient public transport use, walking and cycling;
 - (j) Development is supported by necessary community facilities, open space and recreational areas and appropriate infrastructure to meet the needs of the local community;

- (k) Non-residential development may be supported in new residential areas where such uses directly support the day to day needs of the immediate residential community;
- (l) Development takes account of the environmental constraints of the land; and
- (m) Any unavoidable impacts are minimised through location, design, operation and management requirements.

6.2.6.3 Criteria for assessment

Table 6.2.6.3A—Low density residential zone code - For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
Height			
PO1 Building height takes into consideration and respects the following: (a) the height of existing buildings on adjoining premises; (b) the development potential, with respect to height, on adjoining premises; (c) the height of buildings in the vicinity of the site; (d) access to sunlight and daylight for the site and adjoining sites; (e) privacy and overlooking; and (f) site area and street frontage length.	AO1 Development has a maximum building height of: (a) 8.5 metres; and (b) 2 storeys above ground level.		Complies AO1 The proposal is for residential development. The dual occupancy designs comply with height requirements.
Outbuildings and residential scale			
PO2 Domestic outbuildings: (a) do not dominate the lot on which they are located; and (b) are consistent with the scale and character of development in the Low-density residential zone.	AO2 Domestic outbuildings do not exceed: (a) 100m² in gross floor area; and (b) 5.5 metres in height above natural ground level.		Not applicable
Siting			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO3 Development is sited in a manner that considers and respects: (a) the siting and use of adjoining premises; (b) access to sunlight and daylight for the site and adjoining sites; (c) privacy and overlooking; (d) opportunities for casual surveillance of adjoining public spaces; (e) air circulation and access to natural breezes; and (f) appearance of building bulk; and (g) relationship with road corridors.	AO3.1 Buildings and structures include a minimum setback of: (a) 6 metres from the primary road frontage; and (b) 3 metres from any secondary road frontage.		Complies PO3 Residential uses will be subject to the setbacks identified in the associated Building Setback Plan submitted with this application. Refer Appendix G . The Plan enables each lot to achieve its full development potential without impacting upon the residential amenity of the use and adjoining lots.
	AO3.2 Buildings and structures include a minimum setback of 2 metres from side and rear boundaries.		Complies PO3 Refer comment PO3.
Accommodation density			
PO4 The density of Accommodation activities: (a) contributes to housing choice and affordability; (b) respects the nature and density of surrounding land use; (c) does not cause amenity impacts beyond the reasonable expectation of accommodation density for the zone; and (d) is commensurate to the scale and frontage of the site.	AO4 Development provides a maximum density for Accommodation activities in compliance with Table 6.2.6.3B .		Complies PO4 Dwelling densities for the proposed dual occupancies range from one dwelling per 265m ² to 362m ² . The proposed density prioritises housing choice and affordability whilst maintaining the low density residential amenity of the area.
Gross floor area			
PO5 Buildings and structures occupy the site in a manner that: (a) makes efficient use of land; (b) is consistent with the bulk and scale of surrounding buildings; and	AO5 Gross floor area does not exceed 600m ² .		Complies AO5 GFA for the dual occupancy designs is compliant.

Performance outcomes	Acceptable outcomes	Complies	Comments
(c) appropriately balances built and natural features.			
For assessable development			
Building design			
PO6 Building facades are appropriately designed to: (a) include visual interest and architectural variation; (b) maintain and enhance the character of the surrounds; (c) provide opportunities for casual surveillance; (d) include a human scale; and (e) encourage occupation of outdoor space.	A06 Buildings include habitable space, pedestrian entrances and recreation space facing the primary road frontage.		Complies PO6 Dual occupancy designs are designed and oriented to maximise residential amenity and safety on site and surrounding.
PO7 Development complements and integrates with the established built character of the Low density residential zone, having regard to: (a) roof form and pitch; (b) eaves and awnings; (c) building materials, colours and textures; and (d) window and door size and location.	A07 No acceptable outcome is provided.		Complies PO7 The proposal is consistent with the low density character of the area.
Non-residential development			
PO8 Non-residential development is only located in new residential areas and: (a) is consistent with the scale of existing development; (b) does not detract from the amenity of nearby residential uses; (c) directly supports the day to day needs of the immediate residential community; and (d) does not impact on the orderly provision of non-	A08 No acceptable outcome is provided.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
residential development in other locations in the shire.			
Amenity			
PO9 Development must not detract from the amenity of the local area, having regard to: (a) noise; (b) hours of operation; (c) traffic; (d) advertising devices; (e) visual amenity; (f) privacy; (g) lighting; (h) odour; and (i) emissions.	AO9 No acceptable outcome is provided.		Complies PO9 The proposal provides for residential development as anticipated for the Low Density Residential Zone and will not detract from the amenity of the local area.
PO10 Development must take into account and seek to ameliorate any existing negative environmental impacts, having regard to: (a) noise; (b) hours of operation; (c) traffic; (d) advertising devices; (e) visual amenity; (f) privacy; (g) lighting; (h) odour; and (i) emissions.	AO10 No acceptable outcome is provided.		Not applicable The proposal is for residential development and there are no known existing environmental impacts. All potential impacts are managed accordingly.

Table 6.2.6.3B—Maximum densities for Accommodation activities

Use	Maximum density
Dual occupancy	1 dwelling per 400m ² of site area
Multiple dwelling	(a) 1 dwelling per 400m² of site area; and (b) 1 bedroom per 200m² of site area.
Residential care facility	1 dwelling or accommodation unit per 250m ² of site area.
Retirement facility	1 dwelling or accommodation unit per 400m ² of site area

8.2.3 Bushfire hazard overlay code

8.2.3.1 Application

- (1) This code applies to assessing development where:
 - (a) land the subject of development is located within a Bushfire hazard area and Potential impact buffer (100 metres) identified on the **Bushfire hazard overlay maps (OM-003a-o)**; and
 - (b) it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

Note—Natural hazards are appropriately reflected in Overlay Maps 3, 6 and 8 and are required to be mapped by State Government in response to Hazard and Safety State Interests.

8.2.3.2 Purpose

- (1) The purpose of the Bushfire hazard overlay code is to minimise the threat of bushfire to people and property.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Development in a Bushfire hazard area is compatible with the nature of the hazard;
 - (b) The number of people and properties subject to bushfire hazards are minimised through appropriate building design and location;
 - (c) Development does not result in a material increase in the extent, duration or severity of bushfire hazard; and
 - (d) Appropriate infrastructure is available to emergency services in the event of a bushfire.

8.2.3.3 Criteria for assessment

Table 8.2.3.3—Bushfire hazard overlay code — For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
Water supply for fire-fighting purposes			
PO1 Development where within a 'Bushfire hazard area' and 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o) maintains the safety of people and property by providing an adequate, accessible and reliable water supply for fire-fighting purposes which is safely located and has sufficient flow and pressure characteristics. Note— A Bushfire hazard management plan must be prepared by suitably qualified persons in seeking to demonstrate compliance with the Performance outcome.	Where within a 'Bushfire hazard area' and 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o) AO1.1 Where in a reticulated water service area, the on-site water supply has flow and pressure characteristics of 10 litres a second at 200 kPa. OR		Complies PO1 Each lot will be provided with reticulated water connections and sufficient flow and pressure characteristics as demonstrated in the Engineering Report included in Appendix E .
	AO1.2 Where access to the reticulated water network is not available, a minimum on site water storage of 5,000 litres is provided that must comprise: (a) a separate tank; or		No applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
	(b) a reserve section in the bottom part of the main water supply tank; or (c) a dam; or (d) a swimming pool. Note—Where a water tank is provided for fire-fighting purposes it is fitted with standard rural fire brigade fittings and the tank is provided with a hardstand area for heavy vehicles.		
For assessable development			
Land use			
PO2 Development within a 'Bushfire hazard area' and 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o) is appropriate to the bushfire hazard risk having regard to the: (a) the bushfire risk compatibility of development; (b) the vulnerability of and safety risk to persons associated with the use; and (c) consequences of bushfire in regard to impacts on essential infrastructure, buildings and structures. Note— A Bushfire hazard management plan must be prepared by suitably qualified persons in seeking to demonstrate compliance with the Performance outcome.	AO2 All buildings, structures, infrastructure and facilities associated with the following uses are located outside any area of the site located within a 'Bushfire hazard area' and a 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o) : (a) child care centre; or (b) community care centre; or (c) correctional facility; or (d) educational establishment; or (e) emergency services; or (f) hospital; or (g) residential care facility; or (h) retirement facility; or (i) rooming accommodation; or (j) shopping centre; or (k) tourist park; or (l) tourist attraction.		Complies AO2 The proposal does not include the stated uses.
Lot design			
PO3 Reconfiguring a lot within a 'Bushfire hazard area' and 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o) minimises the potential adverse impacts of bushfire	Where within a 'Bushfire hazard area' and 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o) AO3.1 No new lots are created.		Complies PO3 The land will undergo clearing and levelling to establish the proposed lots, resulting in the removal of vegetation on site, effectively removing hazardous

Performance outcomes	Acceptable outcomes	Complies	Comments
on the safety of people, property and the environment through lot design that: (a) is responsive to the nature and extent of bushfire risk; and (b) allows efficient emergency access to buildings for fire-fighting appliances. Note— A Bushfire hazard management plan must be prepared by suitably qualified persons in seeking to demonstrate compliance with the Performance outcome.	OR AO3.2 All lots include a building envelope that achieves a radiant heat flux level of 29kW/m² at the perimeter of the building envelope. Note—Where a radiant heat flux of 29kW/m² is achieved and this relies on cleared or maintained land external to the land the subject of the development application it must be demonstrated that land external to the site will be maintained to a standard that does not exceed the level of bushfire hazard identified in a Bushfire hazard management plan.		vegetation and largely eliminating bushfire risk within the site. Bushfire risk is further mitigated as each lot will have lawful access to a gazetted road and be connected to Council's reticulated water supply, allowing for efficient response by emergency services.
Firebreaks and access			
PO4 In a 'Bushfire hazard area' and 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o), vehicular access is designed to mitigate against bushfire hazard by: (a) ensuring adequate access for fire-fighting and other emergency vehicles; (b) ensuring adequate access for the evacuation of residents and emergency personnel in an emergency situation, including alternative safe access routes should access in one direction be blocked in the event of a fire; and (c) providing for the separation of developed areas and adjacent bushland.	AO4.1 In a 'Bushfire hazard area' and 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o), roads are designed and constructed: (a) with a maximum gradient of 12.5%; (b) to not use cul-de-sacs; and (c) a constructed road width and weather standard complying with Planning Scheme Policy 4 - FNQROC Regional Development Manual.		Complies PO4 The use of cul-de-sacs is deemed appropriate given the removal of hazardous vegetation on site and unique shape of the subject site, and short length of each access place (cul-de-sac). Each lot can be adequately accessed by emergency service vehicles and the new road is directly linked to Haren Street and the wider transport network, allowing efficient evacuation if necessary. All roads are in accordance with the FNQROC Development Manual.
	AO4.2 In a 'Bushfire hazard area' and 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps		Complies PO4 Each lot can be adequately accessed by emergency service vehicles and the new road is directly linked

Performance outcomes	Acceptable outcomes	Complies	Comments
Note—Where it is not practicable to provide firebreaks in accordance with A04.2 Fire Maintenance Trails are provided in accordance with the following: i. located as close as possible to the boundaries of the lot and the adjoining hazardous vegetation; ii. the minimum cleared width not less than 6 metres; iii. the formed width is not less than 2.5 metres; iv. the formed gradient is not greater than 15%; v. vehicular access is provided at both ends; vi. passing bays and turning areas are provided for fire-fighting appliances located on public land. Note— A Bushfire hazard management plan must be prepared by suitably qualified persons in seeking to demonstrate compliance with the Performance outcome.	(OM-003a-o), firebreaks are provided: (a) consisting of a perimeter road that separates lots from areas of bushfire hazard; (b) a minimum cleared width of 20 metre; (c) a maximum gradient of 12.5%; and (d) a constructed road width and weather standard complying with Planning Scheme Policy 4 - FNQROC Regional Development Manual.		to Haren Street and the wider transport network, allowing efficient evacuation if necessary. All roads are in accordance with the FNQROC Development Manual.
Hazardous materials			
PO5 Public safety and the environment are not adversely affected by the detrimental impacts of bushfire of hazardous materials manufactured or stored in bulk. Note— A Bushfire hazard management plan must be prepared by suitably qualified persons in seeking to demonstrate compliance with the Performance outcome.	AO5 The processing or storage of dangerous goods or hazardous materials is not undertaken in a 'Bushfire hazard area' and a 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o).		Not applicable The proposal is for residential development only.
Landscaping			
PO6 Landscaping within a 'Bushfire hazard area' and a	AO6 No acceptable outcome is provided.		Can be conditioned to comply

Performance outcomes	Acceptable outcomes	Complies	Comments
'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o) does not result in a material increase in the extent, duration or severity of bushfire hazard having regard to: (a) fire ecology; (b) slope of site; and (c) height and mix of plant species. Note—Frost hollows and the associated grass kill facilitates a rapid curing of fuel and exacerbates bushfire hazard. Note— A Bushfire hazard management plan must be prepared by suitably qualified persons in seeking to demonstrate compliance with the Performance outcome.			
Infrastructure			
PO7 Infrastructure services located in a 'Bushfire hazard area' and a 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o) are protected from damage or destruction in the event of a bushfire. Note— A Bushfire hazard management plan must be prepared by suitably qualified persons in seeking to demonstrate compliance with the Performance outcome.	AO7 The following infrastructure services are located below ground: (a) water supply; (b) sewer; (c) electricity; (d) gas; and (e) telecommunications		Complies AO7 The relevant services to the proposed lots will be located below ground. It is noted that the removal of vegetation onsite will largely eliminate bushfire risk within the site.
Private driveways			
PO8 All premises located in a 'Bushfire hazard area' and a 'Potential impact buffer (100 metres)' identified on the Bushfire hazard overlay maps (OM-003a-o) are provided with vehicular access that enables safe	AO8 Private driveways: (a) do not exceed a length of 60 metres from the street frontage; (b) do not exceed a gradient of 12.5%; (c) have a minimum width of 3.5 metres;		Complies AO8 All proposed lots are provided safe and efficient access. Driveways to battle axe will be provided in accordance with the AO.

Performance outcomes	Acceptable outcomes	Complies	Comments
evacuation for occupants and easy access by fire-fighting appliances. Note— A Bushfire hazard management plan must be prepared by suitably qualified persons in seeking to demonstrate compliance with the Performance outcome.	(d) have a minimum vertical clearance of 4.8 metres; (e) accommodate turning areas for fire-fighting appliances in accordance with the Queensland Fire and Emergency Services' Fire Hydrant and Vehicle Access Guidelines; and (f) serve no more than three dwellings or buildings.		

Accommodation activities code

9.3.1.1 Application

- (1) This code applies to assessing development where:
 - (a) involving Accommodation activities; and
 - (b) it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

9.3.1.2 Purpose

- (1) The purpose of the Accommodation activities code is to facilitate the provision of Accommodation activities in appropriate locations throughout the shire.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Accommodation activities are designed, located and operated to minimise any adverse impacts on the natural environment and amenity of surrounding uses;
 - (b) Accommodation activities in the Centre zone are facilitated where they can integrate and enhance the fabric of the centre and are located behind or above commercial development;
 - (c) Accommodation activities provide a high level of amenity and are reflective of the surrounding character of the area;
 - (d) Accommodation activities are generally established in accessible, well-connected locations with access or future access to public transport, cycling and pedestrian networks;
 - (e) Accommodation activities do not compromise the viability of the hierarchy and network of centres, namely:
 - (i) Mareeba as a major regional activity centre, which accommodates the most significant concentrations of regional-scale business, retail, entertainment, government administration, secondary and tertiary educational facilities and health and social services within the shire;
 - (ii) Kuranda as a village activity centre, which accommodates services, arts and cultural facilities, sports and recreation facilities, business and employment uses to support the village and its constituent surrounding rural and rural residential communities;
 - (iii) Chillagoe and Dimbulah as Rural activity centres, which provide commercial and community services to their rural catchments; and
 - (iv) Bibbohra, Irvinebank, Julatten, Koah, Mutchilba, Mt Molloy, Myola and Speewah as rural villages, that have limited centre activities and other non-residential activities; and
 - (f) Accommodation activities are responsive to site characteristics and employ best practice industry standards.

9.3.1.3 Criteria for assessment

Table 9.3.1.3A—Accommodation activities code – For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
All Accommodation activities, apart from Dwelling house			
PO1 Accommodation activities are located on a site that includes sufficient area: (a) to accommodate all buildings, structures, open space and infrastructure associated with the use; and (b) to avoid adverse impacts on the amenity or privacy of nearby land uses.	AO1 Development is located on a site which provides the applicable minimum site area and minimum road frontage specified in Table 9.3.1.3B .		Complies PO1 All dual occupancies have been designed to be responsive of site constraints, compliant with the proposed Building Setback Plan, and includes sufficient area to accommodate all buildings, structures, open space and infrastructure associated with the use. The intentional siting and design of each dual occupancy will maintain the amenity and privacy of users and surrounding lots.
All Accommodation activities, apart from Tourist park and Dwelling house			
PO2 Accommodation activities are provided with on-site refuse storage areas that are: (a) sufficient to meet the anticipated demand for refuse storage; and (b) appropriately located on the site having regard to potential odour and noise impacts on uses on the site and adjoining sites.	AO2.1 A refuse area is provided that: (a) includes a water connection; (b) is of a size and configuration to accommodate 2x240 litre bins per dwelling or accommodation unit where involving a use other than a residential care facility or retirement facility; and (c) is of a size and configuration to accommodate a minimum of two bulk refuse bins where involving a residential care facility or retirement facility.		Complies AO2.1 Each lot proposed to accommodate a dual occupancy is designed with sufficient area to accommodate appropriate refuse storage. All refuse will be collected via kerbside collection.
All Accommodation activities, except for Dwelling house			
PO3 Accommodation activities are designed to avoid overlooking or loss of privacy for adjoining uses. Note—These provisions apply to any adjoining use,	AO3 The windows of habitable rooms: (a) do not overlook the windows of a habitable room in an adjoining dwelling or accommodation unit; or		Complies PO3 Dual occupancy designs are sited to maintain appropriate separation distances where containing openings to habitable rooms or will otherwise be separated by

Performance outcomes	Acceptable outcomes	Complies	Comments
both on an adjoining site and on the same site.	(b) are separated from the windows of a habitable room in an adjoining dwelling or accommodation unit by a distance greater than: (i) 2 metres at ground level; and (ii) 8 metres above ground level; or (c) are treated with: (i) a minimum sill height of 1.5 metres above floor level; or (ii) fixed opaque glassed installed below 1.5 metres; or (iii) fixed external screens; or (iv) a 1.5 metre high screen fence along the common boundary.		boundary fencing, where located in adjoining lots.
PO4 Accommodation activities are provided with sufficient private and communal open space areas which: (a) accommodate a range of landscape treatments, including soft and hard landscaping; (b) provide a range of opportunities for passive and active recreation; (c) provide a positive outlook and high quality of amenity to residents; (d) is conveniently located and easily accessible to all residents; and (e) contribute to an active and attractive streetscape.	AO4.1 Development, except for Caretaker's accommodation, Dwelling house, Dual occupancy or Home based business, includes communal open space which meets or exceeds the minimum area, dimension and design parameters specified in Table 9.3.1.3C.		Not applicable
	AO4.2 Development includes private open space for each dwelling or accommodation unit which meets or exceeds the minimum area, dimension and design parameters specified in Table 9.3.1.3D.		Complies AO4.2 Dual occupancy designs have been selected for their appropriateness to the site constraints and provide a high level of amenity and access to private open space.
	AO4.3 Clothes drying areas are provided at the side or rear of the site so that they are not visible from the street.		Complies AO4.3 Clothes drying areas are not visible from the street.
	AO4.4 If for Dual occupancy, Multiple dwelling, Residential care		Complies AO4.4 Each dual occupancy design includes and

Performance outcomes	Acceptable outcomes	Complies	Comments
	facility or Retirement facility, development provides a secure storage area for each dwelling or accommodation unit which: (a) is located to facilitate loading and unloading from a motor vehicle; (b) is separate to, and does not obstruct, on-site vehicle parking or manoeuvring areas; (c) has a minimum space of 2.4m² per dwelling or accommodation unit; (d) has a minimum height of 2.1 metres; (e) has minimum dimensions to enable secure bicycle storage; (f) is weather proof; and (g) is lockable.		enclosed garage and adequate storage area.
If for Caretaker's Accommodation			
PO5 Caretaker's accommodation is of a scale and intensity which is consistent with that of the surrounding area. Note—Where Caretaker's Accommodation is assessable development additional assessment benchmarks are provided under "for assessable development".	AO5.1 Only one caretaker's accommodation is established on the title of the non-residential use.		Not applicable
	AO5.2 In the Rural zone, Caretaker's accommodation has a maximum gross floor area of 200m ² .		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
If for Dwelling house			
PO6 Where a Dwelling house involves a secondary dwelling, it is designed and located to: (a) not dominate the site; (b) remain subservient to the primary dwelling; and (c) be consistent with the character of the surrounding area;	AO6.1 The secondary dwelling is located within: (a) 10 metres of the primary dwelling where on a lot that has an area of 2 hectares or less; or (b) 20 metres of the primary dwelling where on a lot that has an area of greater than 2 hectares.		Not applicable
	AO6.2 A secondary dwelling has a maximum gross floor area of 100m ² .		Not applicable
If for Dual occupancy			
PO7 Where establishing a Dual occupancy on a corner lot, the building is designed to: (a) maximise opportunities for casual surveillance; (b) provide for separation between the two dwellings; and (c) provide activity and visual interest on both frontages.	AO7.1 Where located on a corner allotment, each dwelling is accessed from a different road frontage.		Complies PO7 The only instance of dual occupancy on a corner lot is within proposed Lot 38. Casual surveillance to each frontage is encouraged with habitable rooms and outdoor living areas opening to either street frontage.
	AO7.2 The maximum width of garage or carport openings that face a public street is 6 metres or 50% of the building width, whichever is the lesser.		Complies PO7 Carport openings facing a public street are not more than 50% of the building width. The frontage can be reasonably activated by surrounding residential uses.
If for Multiple dwelling, Residential care facility or Retirement facility			
PO8 Development is appropriately located within the Shire to: (a) maximise the efficient utilisation of existing infrastructure, services and facilities; and (b) minimise amenity impacts through the collocation of compatible uses. Note—Where Residential care facility or Retirement facility is assessable	AO8 Multiple dwelling, Residential care facility or Retirement facility uses are located on land within 800 metres of the boundary of land within the Centre zone.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
development additional assessment benchmarks are provided under “for assessable development”.			
PO9 Buildings are designed to: (a) reduce the appearance of building bulk; (b) provide visual interest through articulation and variation; (c) be compatible with the embedded, historical character for the locality; and (d) be compatible with the scale of surrounding buildings Note—Where Residential care facility or Retirement facility is assessable development additional assessment benchmarks are provided under “for assessable development”.	AO9.1 External walls do not exceed 10 metres in continuous length unless including a minimum of three of the following building design features and architectural elements: (a) a change in roof profile; or (b) a change in parapet coping; or (c) a change in awning design; or (d) a horizontal or vertical change in the wall plane; or (e) a change in the exterior finishes and exterior colours of the development.		Not applicable
	AO9.2 For a Multiple dwelling, Residential care facility or Retirement facility, the maximum width of a garage or carport opening that faces a road is 6 metres.		Not applicable
	AO9.3 For a Multiple dwelling, Residential care facility or Retirement facility, the building(s) include awnings with a minimum overhang of 600mm.		Not applicable
	AO9.4 For a Multiple dwelling, Residential care facility or Retirement facility, roof forms include one or more of the following types: (a) pyramidal; (b) hip or hipped; (c) gable; (d) skillion.		Not applicable
If for Residential care facility or Retirement facility			
PO10 The layout and design of the site: (a) promotes safe and easy pedestrian, cycle	AO10.1 The development incorporates covered walkways and ramps on site for weather protection between all buildings.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
and mobility device movement; (b) defines areas of pedestrian movement; and (c) assists in navigation and way finding. Note—Where Residential care facility or Retirement facility is assessable development additional assessment benchmarks are provided under “for assessable development”.	AO10.2 Pedestrian paths include navigational signage at intersections.		Not applicable
	AO10.3 Buildings, dwellings and accommodation units include identification signage at entrances.		Not applicable
	AO10.4 An illuminated sign and site map is provided at the main site entry.		Not applicable
	AO10.5 Buildings, structures and pathways associated with a Residential care facility or Retirement facility are not located on land with a gradient greater than 8%.		Not applicable
If for Home based business			
PO11 Home based businesses are compatible with the built form, character and amenity of the surrounding area, having regard to: (a) size and scale; (b) intensity and nature of use; (c) number of employees; and (d) hours of operation.	AO11.1 The Home based business is located within a dwelling house or outbuilding associated with a dwelling house.		Not applicable
	AO11.2 The Home based business does not occupy a gross floor area of more than 50m ² .		Not applicable
	AO11.3 No more than 1 person (other than the residents of the site) is employed by the Home based business at any one time.		Not applicable
	AO11.4 The Home based business, unless a home office, bed and breakfast or farm stay, does not operate outside the hours of 7.00 am and 6.00 pm.		Not applicable
	AO11.5 The Home based business does not involve the public display of goods external to the building.		Not applicable
	AO11.6 The Home based business does not involve the repair, cleaning or servicing of any motors, vehicles or other machinery.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
	AO11.7 Any equipment or materials associated with the Home based business are screened from public view and adjacent properties by fencing or landscaping.		Not applicable
	AO11.8 The business does not involve the use of power tools or similar noise generating devices.		Not applicable
PO12 Home based businesses involving accommodation activities are appropriately scaled and designed to avoid detrimental impacts on the amenity and privacy of surrounding residences.	AO12.1 Home based businesses involving accommodation activities are limited to the scale specified in Table 9.3.1.3E .		Not applicable
	AO12.2 A farm stay dwelling or accommodation unit is located within 20 metres of the primary dwelling house.		Not applicable
	AO12.3 A farm stay is setback 100 metres from any property boundary.		Not applicable
	AO12.4 Entertainment and dining facilities associated with an accommodation activity are: (a) located at least 5 metres from the bedrooms of adjoining residences; and (b) located or screened so that they do not directly overlook private open space areas of adjoining properties.		Not applicable
If for Rural workers' accommodation			
PO13 The Rural workers' accommodation is directly associated with an agricultural based rural activity on the same premises and is commensurate with the scale of agricultural operations.	AO13.1 A Rural workers' accommodation building is limited to the accommodation of: (a) one rural worker for every 50 hectares; and (b) a maximum of ten rural workers in total.		Not applicable
	AO13.2 The agricultural based rural activity is a minimum of 50 hectares in area.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
PO14 Rural workers' accommodation is provided with amenities commensurate with the: (a) needs of the employees; and (b) permanent or seasonal nature of the employment.	AO14.1 The Rural workers' accommodation is: (a) for permanent occupation; and (b) fully self-contained. OR		Not applicable
	AO14.2 The Rural workers' accommodation: (a) is for seasonal occupation (up to 3 months); (b) shares facilities with an existing Dwelling house or Caretaker's residence; and (c) is located within 100 metres of the Dwelling house or Caretaker's residence.		Not applicable
For assessable development			
If for Caretaker's Accommodation			
PO15 The inclusion of Caretaker's accommodation on the site is necessary for the operation of the primary use, having regard to: (a) hours of operation; (b) nature of the use; (c) security requirements; (d) site location and access; and (e) proximity to other land uses.	AO15 No acceptable outcome is provided.		Not applicable
If for Residential care facility or Retirement facility			
PO16 Retirement facilities include a range of housing designs and types that: (a) meet the needs of residents; (b) allow for 'ageing in place'; (c) consider differing mobility needs; (d) accommodate differing financial situations; and (e) cater for different household types.	AO16 No acceptable outcome is provided.		Not applicable
If for Tourist park			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO17 The Tourist park is appropriately located to provide park users with convenient access to tourist attractions, community facilities and infrastructure.	AO17 No acceptable outcome is provided.		Not applicable
PO18 The density of accommodation provided within the Tourist park: (a) is commensurate with the size and utility of the site; (b) is consistent with the scale and character of development in the surrounding area; (c) ensures sufficient infrastructure and services can be provided; (d) does not adversely impact on the existing amenity of nearby uses; (e) ensures a high level of amenity is enjoyed by residents of the site; and (f) does not place undue pressure on environmental processes in the surrounding area.	AO18.1 Where park areas are proposed to exclusively accommodate caravans, motor homes, tents or cabins, accommodation site densities do not exceed: (a) 40 caravan or motor home sites per hectare of the nominated area(s); or (b) 60 tent sites per hectare of the nominated area(s); or (c) 10 cabins (maximum 30m ² gross floor area per cabin) per hectare of the nominated area(s).		Not applicable
	AO18.2 Where park areas are proposed to be used for any combination of caravans, motor homes, tents or cabins, then the lowest applicable density identified by AO18.1 shall be applied to the nominated area(s).		Not applicable
PO19 Accommodation sites are designed and located: (a) to provide sufficient land for necessary services and infrastructure; (b) to achieve sufficient separation between land uses; (c) is consistent with the scale and character of development in the surrounding area; and (d) to prevent amenity and privacy impacts on nearby land uses.	AO19.1 A minimum of 50% of provided caravan and motor home accommodation sites have a concrete slab with a minimum length of 6 metres and a minimum width of 2.4 metres.		Not applicable
	AO19.2 Caravan, motor home, tent and cabin accommodation sites are set back a minimum of: (a) 2 metres from an internal road; and (b) 1.5 metres from the side and rear boundaries of the site.		Not applicable
PO20	AO20.1		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
A Tourist park is provided with sufficient and appropriately located refuse collection areas.	A central refuse collection area is provided to service all accommodation sites.		
	AO20.2 The refuse collection area must be kept in a sanitary condition at all times with all refuse stored in weather-proof and securable receptacles to prevent them from attracting vermin and wildlife.		Not applicable
	AO20.3 The refuse collection area is constructed on an impervious surface such as a concrete slab.		Not applicable
	AO20.4 A water connection is provided within the refuse collection area to facilitate cleaning of receptacles and the collection area.		Not applicable
	AO20.5 Refuse collection areas are located a minimum of 10 metres from any recreational areas, communal cooking facilities and accommodation sites.		Not applicable

9.4.3 Parking and access code

9.4.3.1 Application

This code applies to assessing development where it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

9.4.3.2 Purpose

- (1) The purpose of the Parking and access code is to ensure:
 - (a) parking areas are appropriately designed, constructed and maintained;
 - (b) the efficient functioning of the development and the local road network; and
 - (c) all development provides sufficient parking, loading/service and manoeuvring areas to meet the demand generated by the use.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Land uses have a sufficient number of parking and bicycle spaces designed in a manner to meet the requirements of the user;
 - (b) Parking spaces and associated manoeuvring areas are safe, functional and provide equitable access;
 - (c) Suitable access for all types of vehicles likely to utilise a parking area is provided in a way that does not compromise the safety and efficiency of the surrounding road network;
 - (d) Premises are adequately serviced to meet the reasonable requirements of the development; and
 - (e) End of trip facilities are provided by new major developments to facilitate alternative travel modes.

9.4.3.3 Criteria for assessment

Table 9.4.3.3A—Parking and access code – For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
Car parking spaces			
PO1 Development provides sufficient car parking to accommodate the demand likely to be generated by the use, having regard to the: (a) nature of the use; (b) location of the site; (c) proximity of the use to public transport services; (d) availability of active transport infrastructure; and (e) accessibility of the use to all members of the community.	AO1 The number of car parking spaces provided for the use is in accordance with Table 9.4.3.3B . Note—Car parking spaces provided for persons with a disability are to be considered in determining compliance with AO1.		Complies AO1 The proposal is for the establishment of low density residential development. For lots containing proposed dual occupancies, area is provided to accommodate one space per dwelling plus visitor space.

Performance outcomes	Acceptable outcomes	Complies	Comments
Vehicle crossovers			
PO2 Vehicle crossovers are provided to:: (a) ensure safe and efficient access between the road and premises; (b) minimize interference with the function and operation of roads; and (c) minimise pedestrian to vehicle conflict.	A02.1 Vehicular access to/from Council roads is designed and constructed in accordance with the Standard drawings in Planning Scheme Policy 4 - FNQROC Regional Development Manual.		Complies A02.1 The proposal is for the establishment of low density residential development. Crossovers will be provided in accordance the FNQROC Development Manual at the time of dwelling construction.
	A02.2 Development on a site with two or more road frontages provides vehicular access from: (a) the primary frontage where involving Community activities or Sport and recreation activities, unless the primary road frontage is a State-controlled road; or (b) from the lowest order road in all other instances.		Complies A02.2 The proposal is for the establishment of low density residential development. Access to the proposed lots will be via the new internal road network.
	A02.3 Vehicular access for particular uses is provided in accordance with Table 9.4.3.3E .		Complies A02.3 .
PO3 Access, manoeuvring and car parking areas include appropriate pavement treatments having regard to: (a) the intensity of anticipated vehicle movements; (b) the nature of the use that they service; and (c) the character of the surrounding locality.	A03 Access, manoeuvring and car parking areas include pavements that are constructed in accordance with Table 9.4.3.3C .		Complies A03

Performance outcomes	Acceptable outcomes	Complies	Comments
For assessable development			
Parking area location and design			
PO4 Car parking areas are located and designed to: (a) ensure safety and efficiency in operation; and (b) be consistent with the character of the surrounding locality.	AO4.1 Car parking spaces, access and circulation areas have dimensions in accordance with AS/NZS 2890.1 Off-street car parking.		Complies AO4.1 Refer comment AO2.2.
	AO4.2 Disabled access and car parking spaces are located and designed in accordance with AS/NZS 2890.6 Parking facilities - Off-street parking for people with disabilities.		Not applicable No disabled access and car parking spaces are required.
	AO4.3 The car parking area includes designated pedestrian routes that provide connections to building entrances.		Complies AO4.3 Dual occupancy garages are provided with direct connections to the dwellings.
	AO4.4 Parking and any set down areas are: (a) wholly contained within the site; (b) visible from the street where involving Commercial activities, Community activities, Industrial activities or a use in the Recreation and open space zone; (c) are set back behind the main building line where involving a Dual occupancy, Multiple dwelling, Residential care facility or Retirement facility; and (d) provided at the side or rear of a building in all other instances.		Complies PO4 Carparking associated with the dual occupancies ensure safety and efficiency in operation and is consistent with the surrounding locality.

Performance outcomes	Acceptable outcomes	Complies	Comments
Site access and manoeuvring			
PO5 Access to, and manoeuvring within, the site is designed and located to: (a) ensure the safety and efficiency of the external road network; (b) ensure the safety of pedestrians; (c) provide a functional and convenient layout; and (d) accommodate all vehicles intended to use the site.	AO5.1 Access and manoeuvrability is in accordance with : (a) AS28901 – Car Parking Facilities (Off Street Parking); and (b) AS2890.2 – Parking Facilities (Off-street Parking) Commercial Vehicle Facilities. Note—Proposal plans should include turning circles designed in accordance with AP34/95 (Austroads 1995) Design Vehicles and Turning Path Templates.		Complies AO5.1 Access and maneuvering is provided in accordance with the relevant standard.
	AO5.2 Vehicular access has a minimum sight distance in accordance with Part 5 of AUSTROADS.		Complies AO5.2 Access is provided in accordance with the relevant standard.
	AO5.3 Vehicular access is located and designed so that all vehicles enter and exit the site in a forward gear.		Complies AO5.3 Access handles within proposed Lots 18, 22, and 35 allow entry/exit is a forward direction.
	AO5.4 Pedestrian and cyclist access to the site: (a) is clearly defined; (b) easily identifiable; and (c) provides a connection between the site frontage and the entrance to buildings and end of trip facilities (where provided).		Not applicable
PO6 Development that involves an internal road network ensures that it's design: (a) ensure safety and efficiency in operation;	AO6.1 Internal roads for a Tourist park have a minimum width of: (a) 4 metres if one way; or (b) 6 metres if two way.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
(b) does not impact on the amenity of residential uses on the site and on adjoining sites, having regard to matters of: (i) hours of operation; (ii) noise (iii) light; and (iv) odour; (c) accommodates the nature and volume of vehicle movements anticipated to be generated by the use; (d) allows for convenient access to key on-site features by pedestrians, cyclists and motor vehicles; and (e) in the Rural zone, avoids environmental degradation.	AO6.2 For a Tourist park, internal road design avoids the use of cul-de-sacs in favour of circulating roads, where unavoidable, cul-de-sacs provide a full turning circle for vehicles towing caravans having: (a) a minimum approach and departure curve radius of 12 metres; and (b) a minimum turning circle radius of 8 metres.		Not applicable
	AO6.3 Internal roads are imperviously sealed and drained, apart from those for an Energy and infrastructure activity or Rural activity.		Not applicable
	AO6.4 Speed control devices are installed along all internal roads, apart from those for an Energy and infrastructure activity or Rural activity, in accordance with Complete Streets.		Not applicable
	AO6.5 Internal roads, apart from those for an Energy and infrastructure activity or Rural activity, are illuminated in accordance with AS 4282 (as amended) - Control of Obtrusive effects of outdoor lighting.		Not applicable
	AO6.6 Where involving an accommodation activity, internal roads facilitate unobstructed access to every dwelling, accommodation unit, accommodation site and building by emergency services vehicles.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
	AO6.7 For an Energy and infrastructure activity or Rural activity, internal road gradients: (a) are no steeper than 1:5; or (b) are steeper than 1:5 and are sealed.		Not applicable
Servicing			
PO7 Development provides access, maneuvering and servicing areas on site that: (a) accommodate a service vehicle commensurate with the likely demand generated by the use; (b) do not impact on the safety or efficiency of internal car parking or maneuvering areas; (c) do not adversely impact on the safety or efficiency of the road network; (d) provide for all servicing functions associated with the use; and (e) are located and designed to minimise their impacts on adjoining sensitive land uses and streetscape quality.	AO7.1 All unloading, loading, service and waste disposal areas are located: (a) on the site; (b) to the side or rear of the building, behind the main building line; (c) not adjacent to a site boundary where the adjoining property is used for a sensitive use.		Not applicable The proposal is for the creation of residential development that will be serviced by kerbside waste collection.
	AO7.2 Unloading, loading, service and waste disposal areas allow service vehicles to enter and exit the site in a forward gear.		Not applicable
	AO7.3 Development provides a servicing area, site access and maneuvering areas to accommodate the applicable minimum servicing vehicle specified in Table 9.4.3.3B .		Not applicable The proposal is for the creation of residential development that will be serviced by kerbside waste collection.
Maintenance			
PO8 Parking areas are used and maintained for their intended purpose.	AO8.1 Parking areas are kept and used exclusively for parking and are maintained in a suitable condition for parking and circulation of vehicles.		Not applicable No parking areas are proposed.

Performance outcomes	Acceptable outcomes	Complies	Comments
	AO8.2 All parking areas will be compacted, sealed, drained, line marked and maintained until such time as the development ceases.		Not applicable
End of trip facilities			
PO9 Development within the Centre zone; Industry zone or Emerging community zone provides facilities for active transport users that:	AO9.1 The number of bicycle parking spaces provided for the use is in accordance with Table 9.4.3.3D .		Not applicable
(a) meet the anticipated demand generated from the use; (b) comprise secure and convenient bicycle parking and storage; and (c) provide end of trip facilities for all active transport users.	AO9.2 End of trip facilities are provided in accordance with Table 9.4.3.3D .		Not applicable
If for Educational establishment or Child care centre where involving more than 100 vehicle movements per day or Renewable energy facility, Sport and recreation activities or Tourist park			
PO10 The level of traffic generated by the development on the surrounding local road network must not result in unacceptable impacts on adjacent land and local road users.	AO10 A traffic impact report is prepared by a suitably qualified person that identifies: (a) the expected traffic movements to be generated by the facility; (b) any associated impacts on the road network; and (c) any works that will be required to address the identified impacts.		Not applicable
If for Educational establishment or Child care centre where involving more than 100 vehicle movements per day or Renewable energy facility, Sport and recreation activities or Tourist park			

Performance outcomes	Acceptable outcomes	Complies	Comments
PO11 The level of traffic generated by the development on the surrounding local road network must not result in unacceptable impacts on adjacent land and local road users.	AO11 A traffic impact report is prepared by a suitably qualified person that identifies: (d) the expected traffic movements to be generated by the facility; (e) any associated impacts on the road network; and (f) any works that will be required to address the identified impacts.		Not applicable

9.4.4 Reconfiguring a lot code

9.4.4.1 Application

- (1) This code applies to assessing development where:
 - (a) for Reconfiguring a lot; and
 - (b) it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

9.4.4.2 Purpose

- (1) The purpose of the Reconfiguring a lot code is to ensure that land is:
 - (a) arranged in a manner which is consistent with the intended scale and intensity of development within the area;
 - (b) provided with access to appropriate movement and open space networks; and
 - (c) contributes to housing diversity and accommodates a range of land uses.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Subdivision of land achieves the efficient use of land and the efficient provision of infrastructure and transport services;
 - (b) Lots are of a suitable size and shape for the intended or potential use having regard to the purpose and overall outcomes of the relevant zone or precinct.
 - (c) Subdivision of land creates lots with sufficient area and dimensions to accommodate the ultimate use, meet user requirements, protect environmental features and account for site constraints;
 - (d) A range and mix of lot sizes is provided to facilitate a variety of industry and housing types;
 - (e) Subdivision design incorporates a road network that provides connectivity and circulation for vehicles and provide safe and efficient access for pedestrians, cyclists and public transport;
 - (f) Subdivision design provides opportunities for walking and cycling for recreation and as alternative methods of travel;
 - (g) Subdivision of land provides and integrates a range of functional parkland, including local and district parks and open space links for the use and enjoyment of the residents of the locality and the shire;
 - (h) Subdivision of land contributes to an open space network that achieves connectivity along riparian corridors and between areas with conservation values;
 - (i) Subdivision within the Rural zone maintains lots equal to or larger than 60ha, except for where:
 - (a) The subdivision results in no additional lots (boundary realignment) and does not create an additional *rural lifestyle* lot or *rural residential purposes* lot; or
 - (b) The subdivision is limited to the creation of one additional allotment to accommodate a *public reconfiguration purpose*;
 - (j) Land in historical townships is not reconfigured to be used for urban purposes; and
 - (k) Residential subdivision and greenfield development is designed to consider and respect:
 - i. topography;
 - ii. climate responsive design and solar orientation;
 - iii. efficient and sustainable infrastructure provision;
 - iv. environmental values;
 - v. water sensitive urban design;
 - vi. good quality agricultural land; and
 - vii. the character and scale of surrounding development.

9.4.4.3 Criteria for assessment

Table 9.4.4.3A—Reconfiguring a lot code – For assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
Area and frontage of lots – except for Rural zone			
PO1 Lots include an area and frontage that: (a) is consistent with the design of lots in the surrounding area; (b) allows the desired amenity of the zone to be achieved; (c) is able to accommodate all buildings, structures and works associated with the intended land use; (d) allow the site to be provided with sufficient access; (e) considers the proximity of the land to: (i) centres; (ii) public transport services; and (iii) open space; and (f) allows for the protection of environmental features; and (g) accommodates site constraints.	AO1.1 Lots provide a minimum area and frontage in accordance with Table 9.4.4.3B .		Complies PO1 The proposed reconfiguration results in lot areas ranging from 450m ² to 925m ² . The proposed reconfiguration results in a total of six (6) rear lots which are considered appropriate to maximise the utilisation of the unique shaped site. It is noted that rear lots 17, 21, 23, and 35 have slightly reduced areas. Lots 20, 29, 32, and 36 contain slightly reduced frontages. All lots are afforded safe access, maintain the low density residential amenity of the area, and can accommodate all elements associated with the intended land use and accommodates site constraints.
Area and frontage of lots – Rural zone			
PO1.1 No lots are created with an area of less than 60ha, except for where: (a) The subdivision results in no additional lots (boundary realignment) and does not create an additional <i>rural lifestyle</i> lot or <i>rural residential purposes</i> lot; or	AO1.1 No acceptable outcome is provided.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
(b) The subdivision is limited to the creation of one additional allotment to accommodate a <i>public reconfiguration purpose</i>. Note: This also applies to applications for boundary realignment.			
PO1.2 Where for a boundary realignment, the realignment only occurs where it would: (a) Improve agricultural efficiency; or (b) Facilitate agricultural activity or conservation outcomes; or (c) Resolve boundary issues where a house, structure or works is built over the boundary line of the lots.	AO1.2 No acceptable outcome is provided.		Not applicable
PO1.3 Where for a boundary realignment, the proposed lots are: (a) Able to accommodate all buildings, structures and works associated with the rural use; (b) Suitable to allow the site to be provided with sufficient access; (c) Include enough space within the new lots to accommodate buffers from adjoining land uses to mitigate adverse impacts such as chemical spray drift, odour, noise, fire, smoke and ash; (d) Do not constrain existing industries from expanding or new agricultural enterprises from being established; (e) Do not create new lots for <i>rural lifestyle</i> or <i>rural residential purposes</i>; and (f) Are not for the purposes of creating a separate house lot.	AO1.3 No acceptable outcome is provided.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
PO1.4 Where for the creation of one additional lot to accommodate a <i>public reconfiguration purpose</i> : (a) The lot has sufficient area to be able to accommodate all buildings, structures and works associated with the intended use; and (b) The intended use commences on the lot prior to its creation, or a statutory covenant is registered on the title restricting the future use of the lot to the intended purpose.	AO1.4 No acceptable outcome is provided.		Not applicable
PO1.5 Reconfiguring a lot that is severed by a gazetted road and that uses the road as the boundary of division only occurs where: (a) The subdivision divides one lot into two; and (b) The existing lot is severed by a road that was gazetted before 9 May 2008; and (c) The resulting lot boundaries use the road as the boundary of division; and (d) The development: (i) facilitates agricultural activity; or (ii) facilitates conservation outcomes; and (e) The development ensures agricultural activity is not compromised.	AO1.5 No acceptable outcome is provided.		Not applicable
PO1.6 All lots include a frontage that allows the site to be provided with sufficient access.	AO1.6 Lots provided a minimum frontage is accordance with Table 9.4.4.3B		Not applicable
Existing buildings and easements			
PO2 Reconfiguring a lot which contains existing land uses or	AO2.1 Each land use and associated infrastructure is		Complies AO2.1 Existing sewer infrastructure is contained within

Performance outcomes	Acceptable outcomes	Complies	Comments
existing buildings and structures ensures:	contained within its individual lot.		Easement A SP298397.
(a) new lots are of sufficient area and dimensions to accommodate existing land uses, buildings and structures; and	AO2.2 All lots containing existing buildings and structures achieve the setback requirements of the relevant zone.		Complies AO2.2 No existing buildings or structures on site.
(b) any continuing use is not compromised by the reconfiguration.			
PO3 Reconfiguring a lot which contains an existing easement ensures:	AO3 No acceptable outcome is provided.		Complies PO3 Each lot encumbered by existing Easement A SP298397 contains sufficient area to site future development without compromising the easement or future structures.
(a) future buildings, structures and accessways are able to be sited to avoid the easement; and			
(b) the reconfiguration does not compromise the purpose of the easement or the continued operation of any infrastructure contained within the easement.			
Boundary realignment			
PO4 The boundary realignment retains all attendant and existing infrastructure connections and potential connections.	AO4 No acceptable outcome is provided.		Not applicable
Access and road network			
PO5 Access to a reconfigured lot (including driveways and paths) must not have an adverse impact on:	AO5 No acceptable outcome is provided.		Complies PO5 All lots are afforded safe and efficient access to a gazetted road and will maintain the amenity of the area and not compromise relevant service provisions.
(a) safety;			
(b) drainage;			
(c) visual amenity;			
(d) privacy of adjoining premises; and			
(e) service provision.			
PO6 Reconfiguring a lot ensures that access to a lot can be provided that:	AO6 Vehicle crossover and access is provided in accordance with the design guidelines and specifications set out in Planning Scheme Policy 4 –		Complies AO6 All lots can be provided lawful access in accordance with the FNQROC Regional Development Manual.
(a) is consistent with that provided in the surrounding area;			

Performance outcomes	Acceptable outcomes	Complies	Comments
(b) maximises efficiency and safety; and (c) is consistent with the nature of the intended use of the lot. Note—The Parking and access code should be considered in demonstrating compliance with PO6.	FNQROC Regional Development Manual.		
PO7 Roads in the Industry zone are designed having regard to: (a) the intended use of the lots; (b) the existing use of surrounding land; (c) the vehicular servicing requirements of the intended use; (d) the movement and turning requirements of B-Double vehicles. Note—The Parking and access code should be considered in demonstrating compliance with PO7.	AO7 No acceptable outcome is provided.		Not applicable
Rear lots			
PO8 Rear lots are designed to: (a) provide a high standard of amenity for residents and other users of the site; (b) provide a high standard of amenity for adjoining properties; and (c) not adversely affect the safety and efficiency of the road from which access is gained.	AO8.1 Rear lots are designed to facilitate development that adjoins or overlooks a park or open space.		Complies PO8 The utilisation of rear lots is responsive to the irregular shaped site boundary and is considered appropriate to maximise the site yield and address the housing demand in Mareeba. Each lot is designed to maintain the amenity of adjoining properties and the safety of the new road.
	AO8.2 No more than two rear lots are created behind any lot with a road frontage.		Complies AO8.2 No more than two rear lots are created behind any lot with a road frontage.
	AO8.3		Complies AO8.3 All access strips exceed 4m in width.

Performance outcomes	Acceptable outcomes	Complies	Comments
	Access to lots is via an access strip with a minimum width of: (a) 4 metres where in the Low density residential zone or Medium density residential zone; or (b) 8 metres otherwise.		
	AO8.4 A single access strip is provided to a rear lot along one side of the lot with direct frontage to the street. Note—Figure A provides further guidance in relation to the desired outcome.		Complies AO8.4 Rear lots are compliant with this outcome.
	AO8.5 No more than 1 in 10 lots created in a new subdivision are rear lots.		Complies PO8 The proposal constitutes six (6) rear lots. The utilisation of rear lots is responsive to the irregular shaped site boundary and is considered appropriate to maximise the site yield and address the housing demand in Mareeba. Each lot is designed to maintain the amenity of adjoining properties and the safety of the new road.
	AO8.6 Rear lots are not created in the Centre zone or the Industry zone.		Complies AO8.6 The site is in the Low Density Residential Zone.
Crime prevention and community safety			
PO9 Development includes design features which enhance public safety and seek to prevent opportunities for crime, having regard to: (a) sightlines; (b) the existing and intended pedestrian movement network; (c) the existing and intended land use pattern; and	AO9 No acceptable outcome is provided.		Complies PO9 The proposed reconfiguration does not impact public safety and is consistent with the intended land use pattern.

Performance outcomes	Acceptable outcomes	Complies	Comments
(d) potential entrapment locations.			
Pedestrian and cycle movement network			
PO10 Reconfiguring a lot must assist in the implementation of a Pedestrian and cycle movement network to achieve safe, attractive and efficient pedestrian and cycle networks.	AO10 No acceptable outcome is provided.		Complies PO10 Pathways can be conditioned accordingly.
Public transport network			
PO11 Where a site includes or adjoins a future public transport corridor or future public transport site identified through a structure planning process, development: (a) does not prejudice the future provision of the identified infrastructure; (b) appropriately treats the common boundary with the future corridor; and (c) provides opportunities to integrate with the adjoining corridor where a it will include an element which will attract pedestrian movement.	AO11 No acceptable outcome is provided.		Not applicable The subject site does not adjoin a future public transport corridor or future public transport site.
Residential subdivision			
PO12 Residential lots are: (a) provided in a variety of sizes to accommodate housing choice and diversity; and (b) located to increase variety and avoid large areas of similar lot sizes.	AO12 No acceptable outcome is provided.		Complies AO12 A variety of lot sizes are included to accommodate housing choice and diversity; and located to increase variety and avoid large areas of similar lot sizes.
Rural residential zone			
PO13 New lots are only created in the Rural residential zone where land is located within the 4,000m ² precinct, the 1 hectare precinct or the 2 hectare precinct.	AO13 No acceptable outcome is provided.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
Additional provisions for greenfield development only			
PO14 The subdivision design provides the new community with a local identity by responding to: (a) site context (b) site characteristics (c) setting (d) landmarks (e) natural features; and (f) views.	AO14 No acceptable outcome provided.		Complies PO14 The subject site is ideally located near the Mareeba centre district and is responsive of site characteristics. The land is relatively unconstrained and does not contain natural features.
PO15 The road network is designed to provide a high level of connectivity, permeability and circulation for local vehicles, public transport, pedestrians and cyclists.	AO15 No acceptable outcome provided.		Complies PO15 It is noted that the site is constrained by the irregular shaped boundaries and by Mareeba Connection Road, being a State controlled road. Therefore, the new road is generally limited to extend from Haren Street only. The use of cul-de-sacs is deemed appropriate given the abovementioned site constraints, and the short lengths of each access place (cul-de-sac). The new road is closely linked to Mareeba Connection Road via Byrnes Street from Haren Street, maintaining connection to the wider transport network to the greatest extent possible.
PO16 The road network is designed to: (a) minimise the number of cul-de-sacs; (b) provide walkable catchments for all residents in cul-de-sacs; and (c) include open cul-de-sacs heads. Note—Figure B provides further guidance in relation to the desired outcome.	AO16 No acceptable outcome provided.		Complies PO16 Refer comment PO15. There are not existing pathways.

Performance outcomes	Acceptable outcomes	Complies	Comments
PO17 Reconfiguring a lot provides safe and convenient access to the existing or future public transport network.	AO17 The subdivision locates 90% of lots within 400 metres walking distance of a future public transport route.		Complies AO17 The subject site is within a 400m walk of the Byrnes Street hail n ride.
PO18 The staging of the lot reconfiguration prioritises delivery of link roads to facilitate efficient bus routes.	AO18 No acceptable outcome provided.		Not applicable The subdivision is not staged.
PO19 Provision is made for sufficient open space to: (a) meet the needs of the occupiers of the lots and to ensure that the environmental and scenic values of the area are protected; (b) retain riparian corridors, significant vegetation and habitat areas and provides linkages between those areas; and (c) meet regional, district and neighbourhood open space requirements.	AO19.1 A minimum of 10% of the site area is dedicated as open space.		Complies PO19 The subject site is within proximity to Centenary Park and does not require additional open space provisions. The new road is approximately 180m away from Centenary Park.
	AO19.2 A maximum of 30% of the proposed open space can consist of land identified as significant vegetation or riparian corridor buffer.		Not applicable
PO20 A network of parks and community land is provided: (a) to support a full range of recreational and sporting activities; (b) to ensure adequate pedestrian, cycle and vehicle access; (c) which is supported by appropriate infrastructure and embellishments; (d) to facilitate links between public open spaces; (e) which is co-located with other existing or proposed community infrastructure; (f) which is consistent with the preferred open space network; and (g) which includes a diversity of settings;	AO20 No acceptable outcome is provided.		Not applicable The subject site is for residential development, and is ideally located in proximity to existing open space and community facilities. No parks and community land is proposed.

9.4.5 Works, services and infrastructure code

9.4.5.1 Application

- (1) This code applies to assessing development where it is identified in the assessment benchmarks for assessable development and requirements for accepted development column of an assessment table in Part 5 of the planning scheme.

9.4.5.2 Purpose

- (1) The purpose of the Works, services and infrastructure code is to ensure that all development is appropriately serviced by physical infrastructure, public utilities and services and that work associated with development is carried out in a manner that does not adversely impact on the surrounding area.
- (2) The purpose of the code will be achieved through the following overall outcomes:
 - (a) Development provides an adequate, safe and reliable supply of potable, fire-fighting and general use water in accordance with relevant standards;
 - (b) Development provides for the treatment and disposal of wastewater and ensures there are no adverse impacts on water quality, public health, local amenity or ecological processes;
 - (c) Development provides for the disposal of stormwater and ensures that there are no adverse impacts on water quality or ecological processes;
 - (d) Development connects to the road network and any adjoining public transport, pedestrian and cycle networks while ensuring no adverse impacts on the safe, convenient and efficient operation of these networks;
 - (e) Development provides electricity and telecommunications services that meet its desired requirements;
 - (f) Development is connected to a nearby electricity network with adequate capacity without significant environment, social or amenity impact;
 - (g) Development does not affect the efficient functioning of public utility mains, services or installations;
 - (h) Infrastructure dedicated to Council is cost effective over its life cycle;
 - (i) Work associated with development does not cause adverse impacts on the surrounding area; and
 - (j) Development prevents the spread of weeds, seeds or other pests.

9.4.5.3 Criteria for assessment

Table 9.4.5.3 - Works, services and infrastructure code – For accepted development subject to requirements and assessable development

Performance outcomes	Acceptable outcomes	Complies	Comments
For accepted development subject to requirements and assessable development			
Water supply			
PO1 Each lot has an adequate volume and supply of water that: (a) meets the needs of users; (b) is adequate for fire-fighting purposes; (c) ensures the health, safety and convenience of the community; and (d) minimises adverse impacts on the receiving environment.	AO1.1 Development is connected to a reticulated water supply system in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual other than where located: (a) in the Conservation zone, Rural zone or Rural residential zone; and (b) outside a reticulated water supply service area.		Complies AO1.1 Development can be effectively connected to the reticulated water supply system in accordance with the FNQROC Regional Development Manual as demonstrated in the engineering report prepared by Neon Consulting included in Appendix E .
	AO1.2 Development, where located outside a reticulated water supply service area and in the Conservation zone, Rural zone or Rural residential zone is provided with: (a) a bore or bores are provided in accordance with the Design Guidelines set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual; or (b) on-site water storage tank/s: (i) with a minimum capacity of 90,000L; (ii) fitted with a 50mm ball valve with a camlock fitting; and (iii) which are installed and connected prior to the occupation or use of the development.		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
Wastewater disposal			
PO2 Each lot provides for the treatment and disposal of effluent and other waste water that: (a) meets the needs of users; (b) is adequate for fire-fighting purposes; (c) ensures the health, safety and convenience of the community; and (d) minimises adverse impacts on the receiving environment.	AO2.1 Development is connected to a reticulated sewerage system in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual other than where located: (a) in the Conservation zone, Rural zone or Rural residential zone; and (b) outside a reticulated sewerage service area.		Complies AO2.1 Development can be effectively connected to the reticulated sewerage supply system in accordance with the FNQROC Regional Development Manual as demonstrated in the engineering report included in Appendix E .
	AO2.2 An effluent disposal system is provided in accordance with ASNZ 1547 On-Site Domestic Wastewater Management (as amended) where development is located: (a) in the Conservation zone, Rural zone or Rural residential zone; and (b) outside a reticulated sewerage service area.		Not applicable
Stormwater infrastructure			
PO3 Stormwater infrastructure is designed and constructed to collect and convey the design storm event to a lawful point of discharge in a manner that mitigates impacts on life and property.	AO3.1 Where located within a Priority infrastructure area or where stormwater infrastructure is available, development is connected to Council's stormwater network in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual.		Complies AO3.1 The subject site is within the Mareeba PIA. Development can be connected to the stormwater network as identified in the engineering report included in Appendix E .
	AO3.2 On-site drainage systems are constructed: (a) to convey stormwater from the premises to a lawful point of discharge; and (b) in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 –		Complies AO3.2 Onsite drainage systems are designed to be constructed to discharge to a lawful point and in accordance with the relevant standards. Refer to the engineering report

Performance outcomes	Acceptable outcomes	Complies	Comments
	FNQROC Regional Development Manual.		included in Appendix E .
Electricity supply			
PO4 Each lot is provided with an adequate supply of electricity	AO4 The premises: (a) is connected to the electricity supply network; or (b) has arranged a connection to the transmission grid; or (c) where not connected to the network, an independent energy system with sufficient capacity to service the development (at near average energy demands associated with the use) may be provided as an alternative to reticulated electricity where: (i) it is approved by the relevant regulatory authority; and (ii) it can be demonstrated that no air or noise emissions; and (iii) it can be demonstrated that no adverse impact on visual amenity will occur.		Complies AO4 Development can be effectively connected to the electricity supply network. Refer the engineering report included in Appendix E .
Telecommunications infrastructure			
PO5 Each lot is provided with an adequate supply of telecommunication infrastructure	AO5 Development is provided with a connection to the national broadband network or telecommunication services.		Complies AO5 Development can be effectively connected to the telecommunications network. Refer the engineering report included in Appendix E .
Existing public utility services			
PO6 Development and associated works do not affect the efficient functioning of public utility mains, services or installations.	AO6 Public utility mains, services are relocated, altered or repaired in association with the works so that they continue to function and satisfy the relevant Design		Complies AO6 Development will not impact upon existing public utility mains. Refer the engineering

Performance outcomes	Acceptable outcomes	Complies	Comments
	Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual.		report included in Appendix E .
Excavation or filling			
PO7 Excavation or filling must not have an adverse impact on the: (a) streetscape; (b) scenic amenity; (c) environmental values; (d) slope stability; (e) accessibility; or (f) privacy of adjoining premises.	AO7.1 Excavation or filling does not occur within 1.5 metres of any site boundary.		Complies PO7 Only minor works are anticipated. Refer the engineering report included in Appendix E .
	AO7.2 Excavation or filling at any point on a lot is to be no greater than 1.5 metres above or below natural ground level.		
	AO7.3 Earthworks batters: (a) are no greater than 1.5 metres in height; (b) are stepped with a minimum width 2 metre berm; (c) do not exceed a maximum of two batters and two berms (not greater than 3.6 metres in total height) on any one lot; (d) have a slope no greater than 1 in 4; and (e) are retained.		
	AO7.4 Soil used for filling or spoil from excavation is not stockpiled in locations that can be viewed from: (a) adjoining premises; or (b) a road frontage, for a period exceeding 1 month from the commencement of the filling or excavation.		Complies AO7.4 Soil will not be stockpiled as described for this outcome.
	AO7.5 All batters and berms to be constructed in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual.		Not applicable
	AO7.6 Retaining walls have a maximum height of 1.5 metres and are designed and constructed in accordance with the Design		Not applicable

Performance outcomes	Acceptable outcomes	Complies	Comments
	Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development manual.		
	AO7.7 Excavation or filling at any point on a lot is to include measures that protect trees at the foot or top of cut or fill batters by the use of appropriate retaining methods and sensitive earth removal or placement and in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development manual.		Complies PO7 Only minor works are anticipated. Refer the engineering report included in Appendix E .
For assessable development			
Transport network			
PO8 The development has access to a transport network of adequate standard to provide for the safe and efficient movement of vehicles, pedestrians and cyclists.	AO8.1 Vehicle access, crossovers, road geometry, pavement, utilities and landscaping to the frontage/s of the site are designed and constructed in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development manual.		Complies AO8.1 All future works can be conditioned to be designed and constructed in accordance with the FNQROC Regional Development manual as part of future operational and access works applications.
	AO8.2 Development provides footpath pavement treatments in accordance with Planning Scheme Policy 9 – Footpath Paving.		Complies AO8.2 Footpaths can be conditioned to comply.
Public infrastructure			
PO9 The design, construction and provision of any infrastructure that is to be dedicated to Council is cost effective over its life cycle and incorporates provisions to minimise adverse impacts.	AO9 Development is in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual.		Complies AO9 Refer the engineering report included in Appendix E .

Performance outcomes	Acceptable outcomes	Complies	Comments
Stormwater quality			
PO10 Development has a non-worsening effect on the site and surrounding land and is designed to: (a) optimise the interception, retention and removal of waterborne pollutants, prior to the discharge to receiving waters; (b) protect the environmental values of waterbodies affected by the development, including upstream, on-site and downstream waterbodies; (c) achieve specified water quality objectives; (d) minimise flooding; (e) maximise the use of natural channel design principles; (f) maximise community benefit; and (g) minimise risk to public safety.	AO10.1 The following reporting is prepared for all Material change of use or Reconfiguring a lot proposals: (a) a Stormwater Management Plan and Report that meets or exceeds the standards of design and construction set out in the Queensland Urban Drainage Manual (QUDM) and the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development Manual; and (b) an Erosion and Sediment Control Plan that meets or exceeds the Soil Erosion and Sedimentation Control Guidelines (Institute of Engineers Australia), including: (i) drainage control; (ii) erosion control; (iii) sediment control; and (iv) water quality outcomes.		Complies AO10.1 Refer the engineering report included in Appendix E .
	AO10.2 For development on land greater than 2,500m ² or that result in more than 5 lots or more than 5 dwellings or accommodation units, a Stormwater Quality Management Plan and Report prepared and certified by a suitably qualified design engineer (RPEQ) is prepared that demonstrates that the development: (a) meets or exceeds the standards of design and construction set out in the Urban Stormwater Quality Planning Guideline and the Queensland Water Quality Guideline; (b) is consistent with any local area stormwater water management planning; (c) accounts for development type, construction phase,		Complies AO10.2 Refer the engineering report included in Appendix E .

Performance outcomes	Acceptable outcomes	Complies	Comments
	(d) local climatic conditions and design objectives; and provides for stormwater quality treatment measures reflecting land use constraints, such as soil type, landscape features (including landform), nutrient hazardous areas, acid sulfate soil and rainfall erosivity.		
PO11 Storage areas for stormwater detention and retention: (a) protect or enhance the environmental values of receiving waters; (b) achieve specified water quality objectives; (c) where possible, provide for recreational use; (d) maximise community benefit; and (e) minimise risk to public safety.	AO11 No acceptable outcome is provided.		Not applicable No detention and retention areas proposed.
Excavation or filling			
PO12 Traffic generated by filling or excavation does not impact on the amenity of the surrounding area.	AO12.1 Haul routes used for transportation of fill to or from the site only use major roads and avoid residential areas.		Complies AO12.1 Noted. Transportation will occur in accordance with this outcome.
	AO12.2 Transportation of fill to or from the site does not occur: (a) within peak traffic times; and (b) before 7am or after 6pm Monday to Friday; (c) before 7am or after 1pm Saturdays; and (d) on Sundays or Public Holidays.		Complies AO12.2 Noted. Transportation will occur in accordance with this outcome.

Performance outcomes	Acceptable outcomes	Complies	Comments
PO13 Air pollutants, dust and sediment particles from excavation or filling, do not cause significant environmental harm or nuisance impacts.	AO13.1 Dust emissions do not extend beyond the boundary of the site.		Complies AO13.1 Noted. Dust emissions will be managed accordingly.
	AO13.2 No other air pollutants, including odours, are detectable at the boundary of the site.		Complies AO13.2 Noted. Pollutants will be managed accordingly.
	AO13.3 A management plan for control of dust and air pollutants is prepared and implemented.		Can be conditioned to comply as part of applications for operational and access works.
PO14 Access to the premises (including driveways and paths) does not have an adverse impact on: (a) safety; (b) drainage; (c) visual amenity; and (d) privacy of adjoining premises.	AO14 Access to the premises (including all works associated with the access): (a) must follow as close as possible to the existing contours; (b) be contained within the premises and not the road reserve, and (c) are designed and constructed in accordance with the Design Guidelines and Specifications set out in the Planning Scheme Policy 4 – FNQROC Regional Development manual.		Can be conditioned to comply as part of applications for vehicular access works.
Weed and pest management			
PO15 Development prevents the spread of weeds, seeds or other pests into clean areas or away from infested areas.	AO15 No acceptable outcome is provided.		Complies PO15 Noted. Pest plants will be managed accordingly.
Contaminated land			
PO16 Development is located and designed to ensure that users and nearby sensitive land uses are not exposed to unacceptable levels of contaminants	AO16 Development is located where: (a) soils are not contaminated by pollutants which represent a health or safety risk to users; or (b) contaminated soils are remediated prior to plan sealing, operational works permit, or issuing of building works permit.		Complies AO16 There are no known contaminated soils within the subject site.

Performance outcomes	Acceptable outcomes	Complies	Comments
Fire services in developments accessed by common private title			
PO17 Fire hydrants are located in positions that will enable fire services to access water safely, effectively and efficiently.	AO17.1 Fire hydrants are located in accessways or private roads held in common private title at a maximum spacing of: (a) 120 metres for residential development; and (b) 90 metres for any other development.		Not applicable No common private title.
	AO17.2 Fire hydrants are located at all intersections of accessways or private roads held in common private title.		Not applicable

APPENDIX E

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2-18 Haren Street, Mareeba

Engineering Services Report

019-2601-R-001 | Revision A

31 July 2026

FS Developments Pty Ltd

ENGINEERING SERVICES REPORT



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Project Name 2-18 Haren Street, Mareeba
Project Address 2-18 Haren Street (Lot 1 on SP298397)
Project No: 019-2601
Document Title: Engineering Services Report
Document No.: 019-2601-R-001
Revision: A
Date: 31/07/2026
Client Name: FS Developments Pty Ltd

Report prepared by
Craig Caplick | Principal Engineer | RPEng RPEQ 25102 | +61 402 568 698 | Craig@ConsultNeon.com.au

A handwritten signature in blue ink, reading 'Craig Caplick', is positioned below the contact information.

Revision History

Rev	Date	Description
A	31/07/2026	Draft

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1. Introduction

This Engineering Services Report, prepared by Neon Consulting accompanies a Development Application for the proposed development at 2-18 Haren Street (Lot 1 on SP298397).

The purpose of this report is to demonstrate that the proposed subdivision can be serviced by existing and proposed infrastructure, identify any engineering constraints affecting the site, and confirm that appropriate servicing solutions are available for future Operational Works.

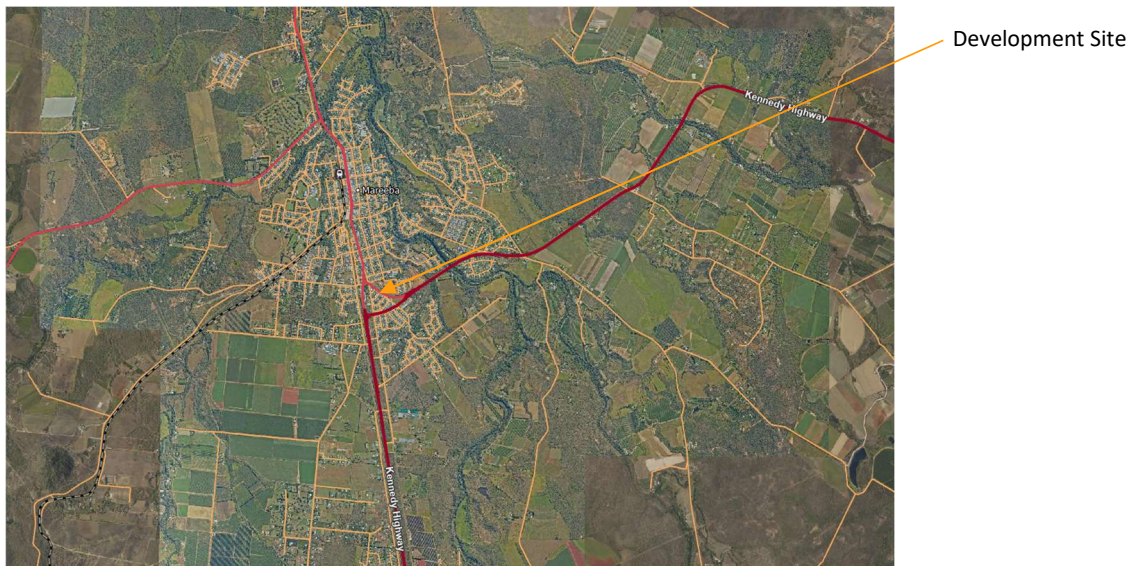


Figure 1 - Locality Aerial Image (image sourced from Qld Globe)

1.1 Disclaimer and Limit of Use

This report has been prepared by Neon Consulting with reasonable skill and care, in accordance with accepted civil engineering practice, and solely for the use of FS Developments Pty Ltd in relation to the 2-18 Haren Street, Mareeba. The assessment, findings, and recommendations are based on information available at the time of writing, including site observations and data provided by the client and external sources. These inputs have been assumed to be accurate unless stated otherwise.

The conclusions in this report are valid only at the time of writing and are specific to the site, scope, and purpose described. They may not remain accurate if site conditions, surrounding development, infrastructure, or regulatory requirements change after the report has been issued. This document does not address matters outside the defined scope of work.

This report must not be copied, distributed, or relied upon by any third party without prior written consent from Neon Consulting. Any unauthorised use is at the sole risk of the user, and Neon Consulting accepts no responsibility or liability for any loss, damage, or consequence arising from such use or from the application of this report beyond its stated purpose or timeframe.

The subject site is located at 2-18 Haren Street (Lot 1 on SP298397), within the Mareeba Shire Council local government area, covers approximately 2.9 hectares. The site is currently vacant and generally free of structures. Existing vegetation comprises several mature trees distributed throughout the site.



Access is primarily via Haren Street, with a site frontage to Antonio Drive and Mareeba Connection Road.

Property Address:	2-18 Haren Street (Lot 1 on SP298397)
Local Government:	Mareeba Shire Council
Local Government Zoning:	Low Density Residential
Local Government Planning Scheme:	Planning Scheme 2016
Registered Site Area:	28930 m ²

ENGINEERING SERVICES REPORT

3. Earthworks, Site Grading and Clearing

The development site is presently undeveloped, with a total area of 2.9ha, and is bounded by the Mareeba Connection Road to south and Haren Street to the north. The concept layout and grading philosophy is such that:

- Each proposed lot can provide a building envelope with a Finished Floor Level (FFL) above the 1% Annual Exceedance Probability (AEP) water surface level, which will be contained within the road reserve.
- The new internal roads alignment generally follows the existing natural topography to provide major stormwater conveyance within the site, with the subgrade founded in cut for a cost-effective road and grading.
- The earthworks proposal is compliant with the FNQROC Development Manual - Design Guideline D2 and stormwater drainage is compliant with FNQROC Development Manual - Design Guideline D4 and the requirements of the Queensland Urban Drainage Manual.
- Provision for future compliance with AS2890.5 – On Street Parking and AS1428 – Design for Access and Mobility internally in the lots.
- Provision of gravity sewer.
- Efficient and economical design

The proposed road alignment generally follows the existing terrain to minimise earthworks, balance cut and fill volumes, maintain gravity sewer grades, and facilitate efficient major overland stormwater flow paths.

Earthwork compaction testing will comply with AS3798 – Guidelines on Earthworks for Commercial and Residential Development and the Far North Queensland Regional Organisation of Councils (FNQROC) Design Guideline D2. Topsoil from the site will be stockpiled before earthworks and spread over the zones identified for grass and landscaping.

3.1 Potential Acid Sulfate Soils

The State Planning Policy Water Quality State Interest Guideline (2016) for Acid Sulfate Soils defines where the State Planning Policy applies. The site has a surface level of much greater than 20 m AHD; therefore, the State Planning Policy does not apply to this development.

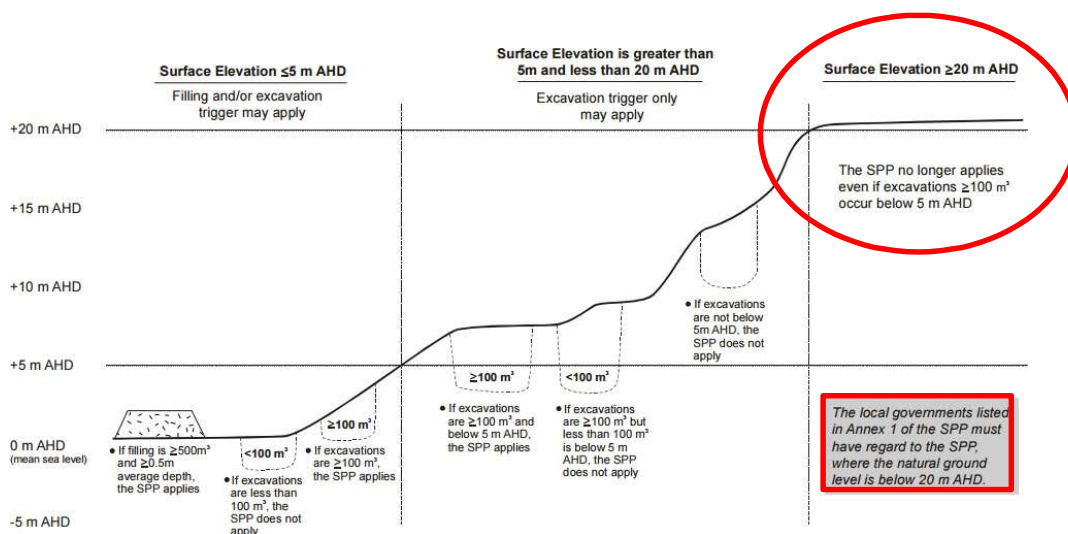


Figure 3 –Extract of State Planning Policy Water Quality State Interest Guideline 2016

3.2 Erosion and Sediment Control

The development will be programmed so that the restoration of ground cover by paving or revegetation is complete within the shortest period of time and by avoiding the tropical wet season. Potential causes of erosion for this site by wind erosion or precipitation are:

- Stripping and removal of topsoil
- Removal of fill
- Other earthwork operations
- Heavy vehicle use on-site

A compliant erosion and sediment control strategy will be provided at the operational works stage to meet the requirements below. The contractor will revise these plans prior to commencing on-site. No clearing is required to be undertaken unless preceded or accompanied by the installation of adequate runoff and sediment control measures.

Following practical completion of the project, a minimum of 70% coverage of all soil with ground cover (i.e. topsoiling and seeding) shall be provided within 30 calendar days.

During the construction phases, water spraying will be used with care to act as a dust suppression method.

3.2.1 Monitoring and Maintenance Programs

Water discharge from the site will adhere to a total suspended solid content of less than 50 milligrams per litre and a pH range of between 6.5 and 8.5 at all times. If the pH of the flocculated water is not achieved, then pH adjustments will be required. This could possibly be done by a dosing of lime.

Site personnel will inspect all erosion and control measures at least at the following frequencies:

- Daily during construction works,
- Weekly when construction works are not happening,
- Within 24 hours of expected rain, and
- Within 18 hours of an impacting rainfall event.

All erosion and sediment control measures that have an order of efficiency below 75% will be corrected by the end of that working day

4. Stormwater Management

4.1 Regional Flooding

The Barron River Basin Level 2 Flood Investigation (2015 Kellogg Brown & Root Pty Ltd) shows that the site is free from the regional flood in a 1% AEP event. Council flood hazard overlays show that the site is free from flooding in the regional (Barron River) flood events.



Figure 4 – Flood Hazard Overlay (image sourced from Mareeba Shire Council Online Mapping)

4.2 Existing Drainage Conditions

Existing stormwater flows are directed from the south west to the north east in this area before eventually joining the Barron River and flowing north. Four (4) culverts discharge from the Mareeba Connection Road towards the site. The northern pair. Approximately located at Johnston Street and Mannino Street intersections, are conveyed to the north of the site before joining the drain on the southern side of Haren Street, which falls to the east. The southern culverts discharge through the site before crossing into Lot 2 on SP298397 and crossing Antonio Drive.

The existing flow regime has been retained wherever practical. The proposed drainage strategy separates external catchment flows from internal development runoff to simplify future servicing of adjoining land while reducing cumulative discharge to downstream properties.



Figure 5 – LiDAR Contours (image sourced from Queensland Globe)

4.3 Development Stormwater Philosophy

The proposed stormwater management strategy has been developed to maintain the existing catchment characteristics wherever practical while providing an orderly drainage network for the proposed subdivision. The design philosophy separates external catchment flows from runoff generated within the development so that each catchment is conveyed independently to an appropriate lawful point of discharge.

Runoff generated from the proposed residential allotments and new road reserves will be collected within the internal drainage network and discharged to the existing drainage system within the Haren Street road reserve. Haren Street therefore forms the lawful point of discharge for the majority of the development in accordance with Queensland Urban Drainage Manual (QUDM) principles.

External catchment flows entering the site from the Mareeba Connection Road are maintained within dedicated drainage corridors and are conveyed through the development generally in accordance with the existing overland flow paths. This approach avoids increasing runoff onto adjoining properties and maintains the existing drainage function of the broader catchment.

The proposed drainage strategy also removes the existing direct discharge of internal development runoff into Lot 2 on SP298397. By directing only the external catchment flows toward Lot 2, future development of that property will only be required to manage its natural upstream catchment rather than a combination of upstream runoff and development-generated flows from this subdivision. This simplifies the future drainage design of Lot 2 and reduces cumulative stormwater discharge entering that property.

The proposed stormwater easement along the rear of proposed Lots 18 to 32 has been adopted in preference to a fully piped drainage system. The easement allows major storm events to be conveyed within an open overland flow path consistent with the natural topography, reducing excavation depths, pipe sizes and long-term maintenance requirements while maintaining a defined drainage corridor. The easement also provides ongoing access for maintenance and preserves the major drainage function independently of future development within the adjoining allotments.

Overall, the proposed stormwater management strategy maintains the existing drainage regime, provides lawful discharge for all internal catchments, avoids worsening downstream impacts and establishes a practical drainage arrangement that integrates with future development of the surrounding land.

The drainage masterplan included in Appendix A proposes to generally maintain the existing flow regime and outlet the site stormwater to the Haren Street drain via the new road reserve and easements.

4.4 Stormwater Quality

For developments in Queensland, the State Planning Policy applies for stormwater quality management or new or expanded non-tidal artificial waterways if any of the following criteria are met;

Criteria	Applies to this development
Material change of use for urban purposes that involves a land area greater than 2,500m ² that will result in an impervious area greater than 25% of the net developable area.	No
Material change of use for urban purposes that involves a land area greater than 2,500m ² that will result in six or more dwellings.	No
Reconfiguring a lot for urban purposes that involves a land area greater than 2,500m ² and will result in six or more lots.	Yes
Operational works for urban purposes that involve disturbing more than 2,500m ² of land.	No

Table 1 – State Planning Policy Assessment Criteria

The State Planning Policy (SPP) for stormwater quality management applies to this development as the site is greater than 2,500m² and it will result in six (6) or more lots.

5. Traffic and Access

A detailed Traffic Impact Assessment has not been undertaken as part of this application. The subject land is identified within the Mareeba Planning Scheme for residential development and the surrounding road network has been planned and constructed to accommodate the ultimate development of the precinct.

Haren Street is identified as an Access Street under the FNQROC Development Manual and Mareeba Planning Scheme road hierarchy. The proposed subdivision of 44 residential lots is consistent with the intended function of this road classification and is not expected to generate traffic volumes requiring an upgrade to the existing road hierarchy. Table D1.1 of the FNQROC Development Manual indicates that an Access Street may service up to approximately 74 dwellings before a higher order road classification is required.

Based on typical residential trip generation rates of approximately 8 to 10 vehicle trips per dwelling per day, the proposed development is expected to generate in the order of 350 to 440 vehicle movements per day. These traffic volumes are consistent with the operational capacity of an Access Street and are well within the design intent of the surrounding residential road network.

Vehicular access has been provided from Haren Street rather than Antonio Drive or the Mareeba Connection Road. Haren Street provides the appropriate road hierarchy for residential access, minimises interaction with higher-speed traffic and results in a safer and more practical access arrangement for residents.

The proposed intersection has been located to achieve appropriate stopping sight distance in accordance with Austroads guidance. Existing road geometry and available sight lines provide suitable visibility for vehicles entering and exiting the subdivision.

Haren Street currently provides approximately 6.5 metres of sealed pavement across the development frontage. As this width is consistent with the requirements for an Access Street, no road widening on the development frontage is considered necessary.



Figure 6 – Existing Haren Street formation (image sourced from Google Maps)

6. Potable and Firefighting Water

The development is located within the municipal water network catchment can be serviced by connection to the Haren Street Water Main WA-002950.

Hydrant spacing, pipe sizing and residual pressures will be confirmed during the detailed Operational Works design in accordance with the FNQROC Development Manual and Queensland Fire Department requirements. Should Council require confirmation of network performance, a hydraulic network analysis can be undertaken prior to Operational Works to demonstrate compliance with minimum residual pressure requirements under peak demand and firefighting conditions.



Figure 7 –Extract of Council Maps Online – Water

The development will extend the existing reticulation network through the subdivision, providing looping of the municipal network and improved operational reliability.

The expected water demand from the development is tabulated below.

Design Criteria	
Lots (401m ² to 900m ²)	44
Demand per Lot	2.8 EP
EP - Equivalent Population	123.2
EDC - Equivalent Domestic Connections	44
Demand per Equivalent Person	500 L/day
AD – Average Daily Demand	61.6 kL/day
MDMM – Mean Day Maximum Month Demand	92.4 kL/day
PD – Peak Day Demand	138.6 kL/day
PH – Peak Hour Demand	3.2 L/s
FF – Fire Flow	15 L/s

Table 2 - Development Water Demand

The concept engineering plans in Appendix A show the proposed location for the potable water network and how this development will provide connectivity across the network. The water demand for the development is not expected to impact the municipal water network, but if required, a water network analysis can be provided prior to Operational Works to verify the development impacts on the surrounding network and ensure that the minimum residual pressure head of 22m is available during Peak Hour consumption and 12m is available during firefighting events.

7. Wastewater Disposal

The proposed subdivision will be serviced by extension of the existing gravity sewer network within the development. The concept design has been developed to maintain gravity servicing to all proposed allotments while making efficient use of the existing sewer easement along the eastern boundary of the site.

Although downstream capacity constraints are understood to exist within parts of the municipal sewer network, the subject land is located within a designated sewerage-benefited area and is appropriately zoned for residential development. The proposed subdivision is therefore consistent with Council's long-term infrastructure planning assumptions, and any future augmentation of the downstream trunk sewer network would normally form part of Council's broader infrastructure planning framework rather than this development.

Detailed sewer design, including pipe grades, maintenance hole locations and property connection locations, will be completed during the Operational Works stage in accordance with the FNQROC Development Manual.



Figure 8 –Extract of Council Maps Online – Sewer

The expected water demand from the development is tabulated below.

Design Criteria	
Lots (401m ² to 900m ²)	44
Generation per Lot	2.8 EP
EP - Equivalent Population	123.2
EDC - Equivalent Domestic Connections	44
Generation per Equivalent Person	270 L/day
C1 Peaking Factor	6.99
C2 Peaking Factor	2.84

ADWF – Average Dry Weather Flow	0.385 L/s
PDWF – Peak Dry Weather Flow	1.092 L/s
PWWF – Peak Wet Weather Flow	2.690 L/s

Table 3 - Development Sewage Generation

The concept engineering plans in Appendix A show the proposed location for the a gravity sewer network within the development which can connect to the existing municipal sewer. Although downstream capacity constraints are understood to exist within parts of the municipal network, the site is located within a sewerage-benefited area and is appropriately zoned for residential development. It is therefore expected that any necessary augmentation of the downstream trunk network will be addressed through Council's infrastructure planning framework.

8. Electricity and Telecommunication

Electricity and telecommunications infrastructure is located nearby within Haren Street. Power and communications will be provided as required by the respective services authorities to service the new lots.

Intent to Supply offers from electrical and telecommunication providers will be provided to Council during the future project phases.

9. Recommendations

Based on the investigations and assessments presented within this Engineering Services Report, the proposed subdivision at 2-18 Haren Street (Lot 1 on SP298397) can be serviced in accordance with the Mareeba Shire Council Planning Scheme, the FNQROC Development Manual, and accepted engineering practice.

The following conclusions are made:

Earthworks and Site Grading

The proposed subdivision layout has been developed to minimise earthworks while providing suitable building pads, compliant road grades, gravity sewer servicing, and appropriate overland stormwater flow paths. Finished floor levels can be achieved above the adopted 1% Annual Exceedance Probability flood level.

Stormwater Management

The site is not affected by regional flooding associated with the Barron River. The proposed drainage strategy maintains the existing catchment philosophy while separating external catchment flows from internal development runoff. Stormwater quality requirements under the State Planning Policy apply and will be addressed through the Site Based Stormwater Management Plan accompanying this application.

Traffic and Access

The proposed subdivision is appropriately served by Haren Street, which functions as an Access Street under the FNQROC Development Manual. The proposed intersection provides safe sight distance and no upgrade to the road hierarchy is required. Internal roads have been designed to provide safe access for residents, visitors, and service vehicles.

Potable Water

The development can be serviced by extending the existing municipal water network within Haren Street. The proposed reticulation provides improved network connectivity and is expected to satisfy domestic and firefighting demands. Any detailed hydraulic assessment can be completed during the Operational Works stage if requested by Council.

Wastewater Disposal

The proposed subdivision can be serviced by extending the municipal gravity sewer network. The proposed layout allows gravity sewer servicing to all allotments while maintaining the existing sewer easement. Any downstream network augmentation will be addressed through Council's infrastructure planning where required.

Electricity and Telecommunications

Electricity and telecommunications infrastructure is available adjacent to the site and can be extended to service the proposed subdivision. Connection arrangements will be confirmed with the relevant service authorities during detailed design.

Overall, the proposed subdivision can be appropriately serviced by existing and planned infrastructure and is considered suitable for approval, subject to standard, relevant, and reasonable conditions.

Appendix A. Concept Engineering Plans





LEGEND

- ACCESS STREET - 6.5m MINIMUM CARRIAGEWAY (0-74 DWELLINGS)
- ASSUMED TRAFFIC CATCHMENT BOUNDARY
- CATCHMENT COUNT (LOTS)
- TOTAL ROAD COUNT (LOTS)

1:1000 0 10 20 30 40

NEON CONSULTING

2-18 HAREN STREET, MAERBA
LOT 1 ON SP298397
MASTERPLANS
ROADWORKS

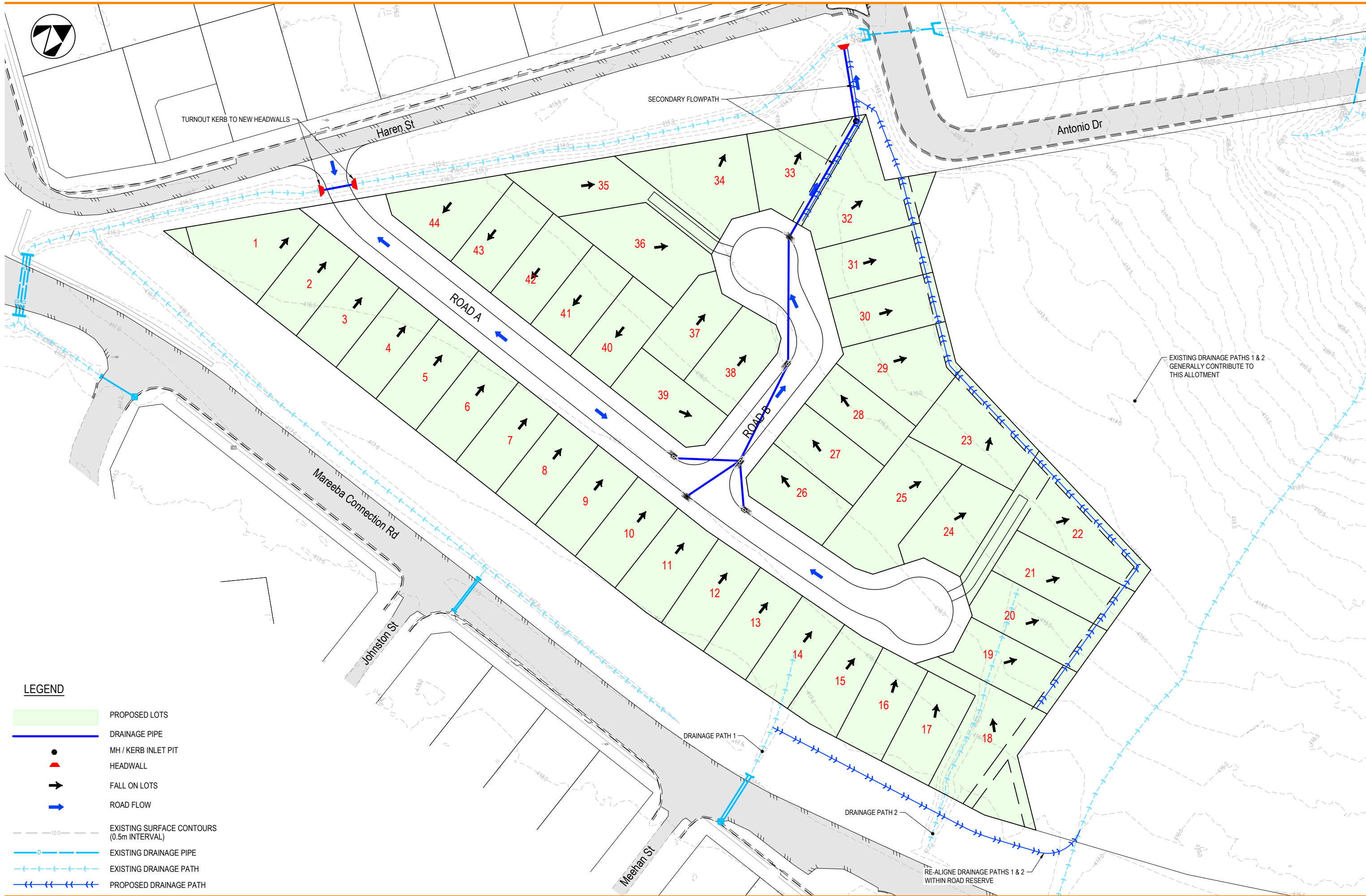
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Drawn PAM
Design PAM
Check'd CJC
Appr'd CJC

A3 Full Size (Scale as shown)
30.07.26

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LEGEND

- PROPOSED LOTS
- DRAINAGE PIPE
- MH / KERB INLET PIT
- HEADWALL
- FALL ON LOTS
- ROAD FLOW
- EXISTING SURFACE CONTOURS (0.5m INTERVAL)
- EXISTING DRAINAGE PIPE
- EXISTING DRAINAGE PATH
- PROPOSED DRAINAGE PATH

1:1000 0 10 20 30 40

NEON
CONSULTING

2-18 HAREN STREET, MAERBA
LOT 1 ON SP298397
MASTERPLANS
DRAINAGE

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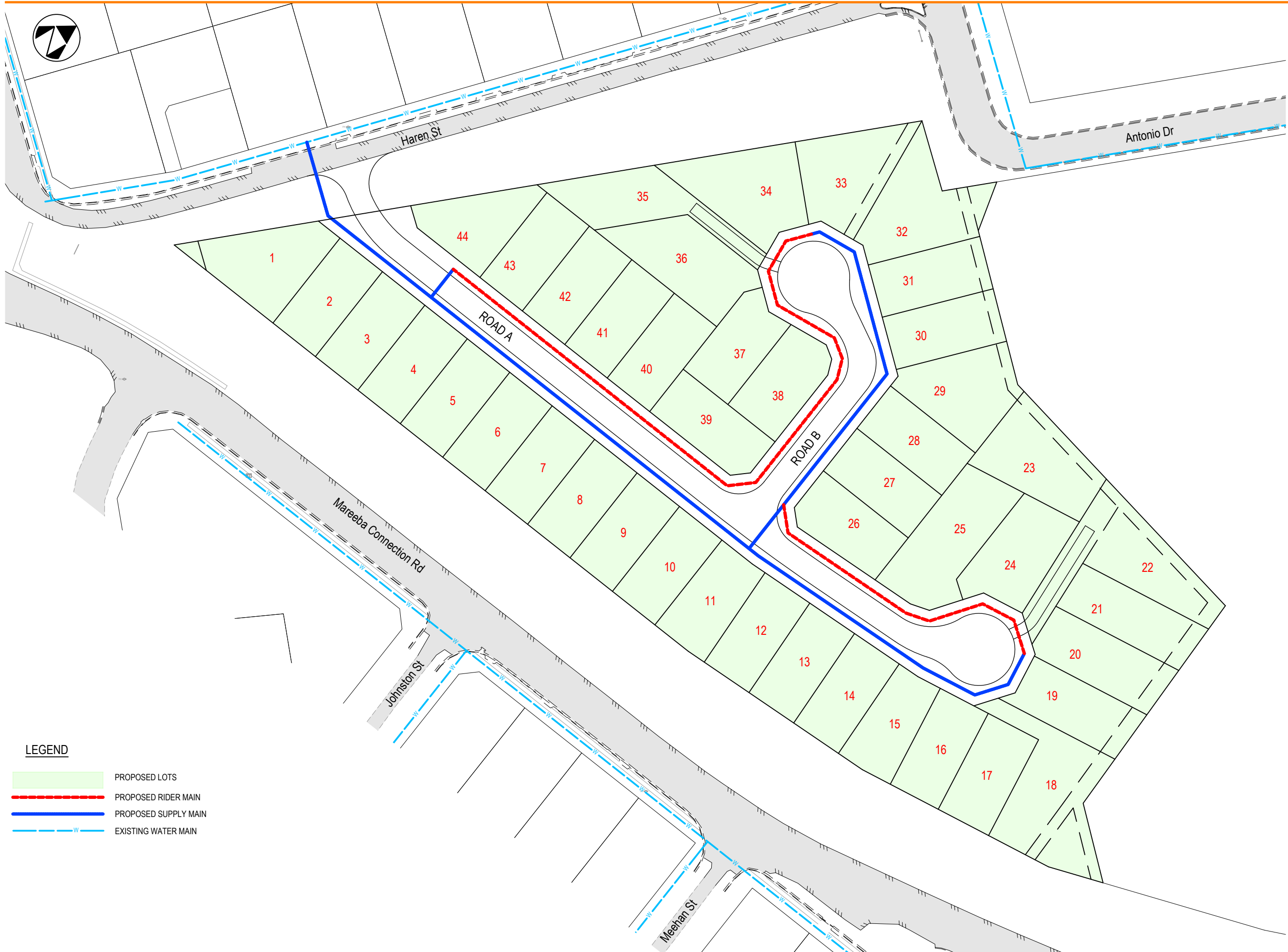
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LEGEND

- PROPOSED LOTS
- PROPOSED RIDER MAIN
- PROPOSED SUPPLY MAIN
- EXISTING WATER MAIN

1:1000 0 10 20 30 40

NEON
CONSULTING

2-18 HAREN STREET, MAERBA
LOT 1 ON SP298397
MASTERPLANS
WATER

A 30.07.26 INITIAL ISSUE

Rev Date Revision Notes

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Drawn	Design	Checked	Approved
PAM	PAM	CJC	CJC

A3 Full Size (Scale as shown)
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APPENDIX F

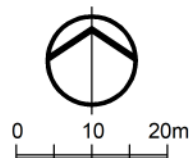
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APPENDIX G

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Building Setback		
	Type 1	Type 2
House / Garage Setback from Road Frontage	4.0m	4.0m (Primary) 2.0m (Secondary)
Side / Rear Setback from Boundary	1.0m	1.0m

— Driveway Exclusion Location (Dimension is from lot corner truncation)

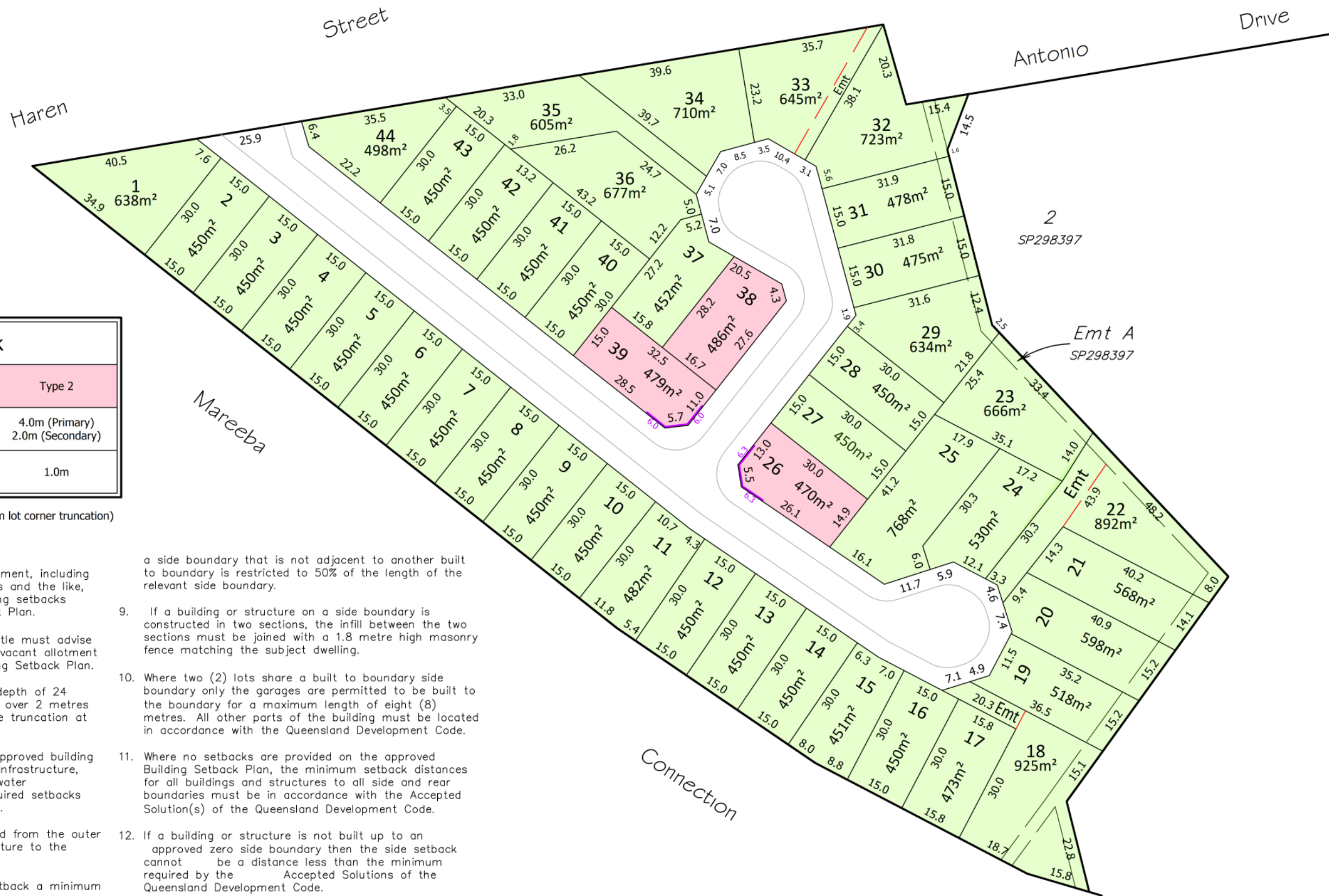
NOTES:

- All building work proposed on each allotment, including any ancillary structures, decks, balconies and the like, shall be located wholly within the building setbacks shown on the approved Building Setback Plan.
- The applicant or their successor(s) in title must advise all prospective purchasers of each new vacant allotment of the existence of the approved Building Setback Plan.
- For corner allotments with an average depth of 24 metres or less, no building or structure over 2 metres in height shall be built within a 9 metre truncation at the corner of two road frontages.
- Where there is a conflict between the approved building setbacks and drainage, sewer or water infrastructure, the position of the drainage, sewer or water infrastructure prevails together with required setbacks from such infrastructure and easements.
- Approved building setbacks are measured from the outer most projection of the building or structure to the relevant boundary.
- All buildings and structures must be setback a minimum of 1.5 metres to completed sewer mains, or as otherwise approved by the Chief Executive Officer.
Note: Deeper sewer mains may require an increased setback.
- Detached two storey dwellings are to have a minimum setback of 2 metres to the relevant boundary unless a greater setback is specified on the approved Building Setback Plan.
- The maximum length of a building or structure built to a side boundary that is not adjacent to another built to boundary is restricted to 50% of the length of the relevant side boundary.
- If a building or structure on a side boundary is constructed in two sections, the infill between the two sections must be joined with a 1.8 metre high masonry fence matching the subject dwelling.
- Where two (2) lots share a built to boundary side boundary only the garages are permitted to be built to the boundary for a maximum length of eight (8) metres. All other parts of the building must be located in accordance with the Queensland Development Code.
- Where no setbacks are provided on the approved Building Setback Plan, the minimum setback distances for all buildings and structures to all side and rear boundaries must be in accordance with the Accepted Solution(s) of the Queensland Development Code.
- If a building or structure is not built up to an approved zero side boundary then the side setback cannot be a distance less than the minimum required by the Accepted Solutions of the Queensland Development Code.
- A driveway and crossover must not be located along the part of a lot which is subject to a Driveway Exclusion Location.
- Access via internal road network only. No vehicular access onto Mareeba Connection Road or Haren Street is permitted.
- Lots with frontage to Mareeba Connection Road and Haren Street is considered the Side / Rear boundary for the purpose of this Building Setback Plan.

This plan is conceptual and for discussion purposes only. All areas, dimensions and land uses are preliminary, subject to investigation, survey, engineering, and Local Authority and Agency approvals.

BUILDING SETBACK PLAN

"HAREN STREET MAREEBA"



Date: 23/07/2026	
Scale: 1:1000	A3
Drawn: WCHO	
Job No: 34692/004-01	
Plan No:	34692/005 B

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TOWNPLANNING
PROJECTMANAGEMENT
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APPENDIX H

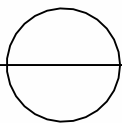
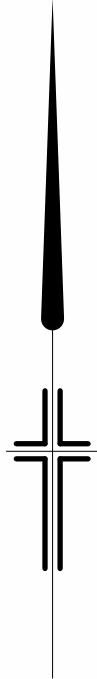
brazier motti



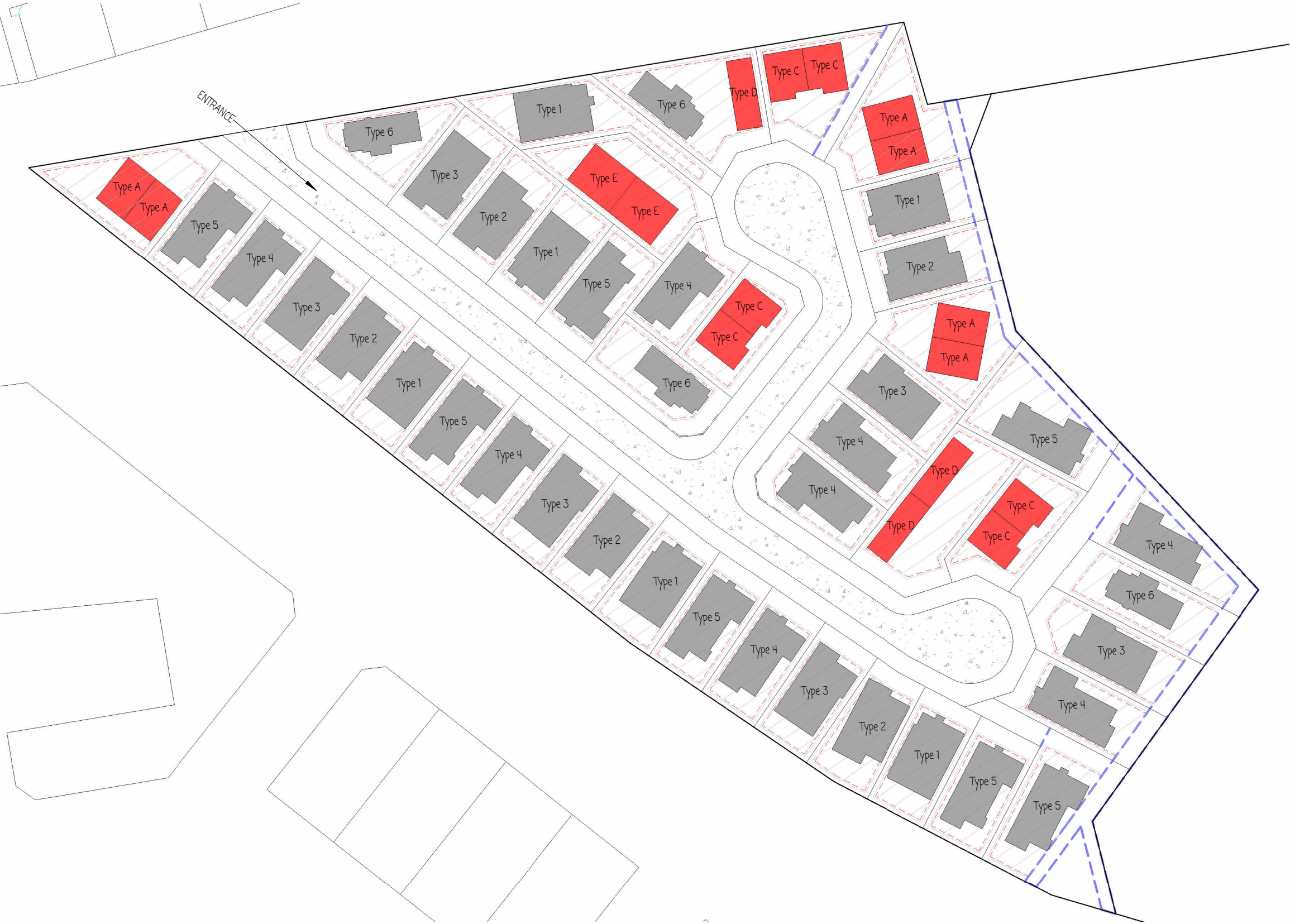
GENERAL NOTES

- REFER SITE PLANS FOR LOCATION, SETOUT AND ACTUAL LEVELS OF BUILDINGS. CONFIRM PRIOR TO EXCAVATION.
- CONTRACTOR TO CHECK ON SITE ALL DIMENSIONS PRIOR TO SHOP DRAWINGS AND FABRICATION.
- ALL DIMENSIONS ARE TO GRID LINES, FACE OF BLOCKWORK/BRICKWORK, FACE OF STUD OR CENTRELINE OF COLUMNS, UNO.
- CONTRACTOR TO CO-ORDINATE ALL SERVICES, PENETRATIONS AND STRUCTURE PRIOR TO CONSTRUCTION AND INFORM THE CONTRACT ADMINISTRATOR PRIOR TO CONSTRUCTION/FABRICATION RAMP, STAIRS, AND PATHWAYS/APRONS TO COMPLY WITH AS14281 (2009).
- WHERE A TRADE NAMED PRODUCT IS SPECIFIED IN THESE DOCUMENTS, IT IS TO BE CONSIDERED AS, 'OR EQUIVALENT TO APPROVAL OF CONTRACT ADMINISTRATOR.'

ASSESSMENT ISSUE



Master Plan
SCALE 1 : 1000



NOTE

NO SEWER PLAN AVAILABLE AT TIME OF DRAWING. VERIFY ON SITE PRIOR TO CONSTRUCTION.
MAX 500KPa WATER PRESSURE OR INSTALL PRESSURE LIMITING DEVICE

SITE NOTES

LICENSED PLUMBER TO CONFIRM FINAL ALIGNMENT OF HOUSE SEWER & STORMWATER. CONFIRM ALL FALLS PRIOR TO CONSTRUCTION.
CLIENT TO PROVIDE SKETCH PLAN SHOWING ANY FUTURE ALTERATIONS, EXTENSIONS, SWIMMING POOLS ETC. SO HOUSE SEWER & STORMWATER CAN BE ALIGNED TO ACCOMMODATE REQUIREMENTS.
ALL PLUMBING & DRAINAGE WORK SHALL BE IN ACCORDANCE WITH SEWERAGE AND WATER SUPPLY ACT 1949-1982 ASSOCIATED AMENDMENTS & RELEVANT AUSTRALIAN STANDARDS.
ALL WATER TO BE DRAINED AWAY FROM BUILDING DURING & AFTER CONSTRUCTION & TO COMPLY WITH AS 2870 'RESIDENTIAL SLABS & FOOTINGS'.
FINISHED SLAB LEVEL TO BE MINIMUM 250mm ABOVE FINISHED GROUND LEVEL.
ALL EARTHWORKS TO COMPLY WITH AS 3798-1996 'GUIDELINES ON EARTHWORKS FOR COMMERCIAL & RESIDENTIAL DEVELOPMENTS'.

ALL EXISTING VEGETATION ON THE PROPERTY WITHIN THE FOOTPRINT OF THE PROPOSED RESIDENCE AND/OR WITHIN A RECOMMENDED SAFE DISTANCE FROM THE PROPOSED RESIDENCE'S FOOTINGS ARE TO BE REMOVED WELL PRIOR TO CONSTRUCTION TO ALLOW THE SOILS MOISTURE CONDITIONS TO RETURN TO A STATE OF EQUILIBRIUM

DEPRESSIONS FORMED BY THE REMOVAL OF VEGETATION & ALL DISTURBED WEAKEND SOIL SHOULD BE CLEANED OUT & BACKFILLED WITH COMPACTED SELECT FILL

LOT NUMBER: ??
RP NUMBER: ??
LOCAL GOV: ??
LOCALITY: ??
SITE AREA:

LEGEND

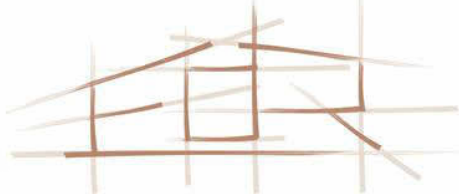
G1	GATE (900 WIDE)
G2	GATE (3000 WIDE)
— — — —	FENCING AS SELECTED
MH	MAN HOLE
WM	WATER METRE
—S—S—S—	EXIST. SEWER
—SW—SW—SW—	EXIST. STORMWATER
—PL—PL—PL—	EXIST. POWER LINES
—W—W—W—	EXIST. WATER
—E—E—E—	EXIST. ELECTRICAL LINE

LOCAL AUTHORITY: ??
ZONE DESCRIPTION: ??
AFFECTED OVERLAYS: ??
??
??
??

ISSUES/REVISIONS



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2016



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-Project Type: ??
-Client Name: ??
-Project Address: ??
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-Project Number: ??
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-Scale: AT A3
-Sheet Number: A-02 I

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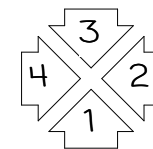
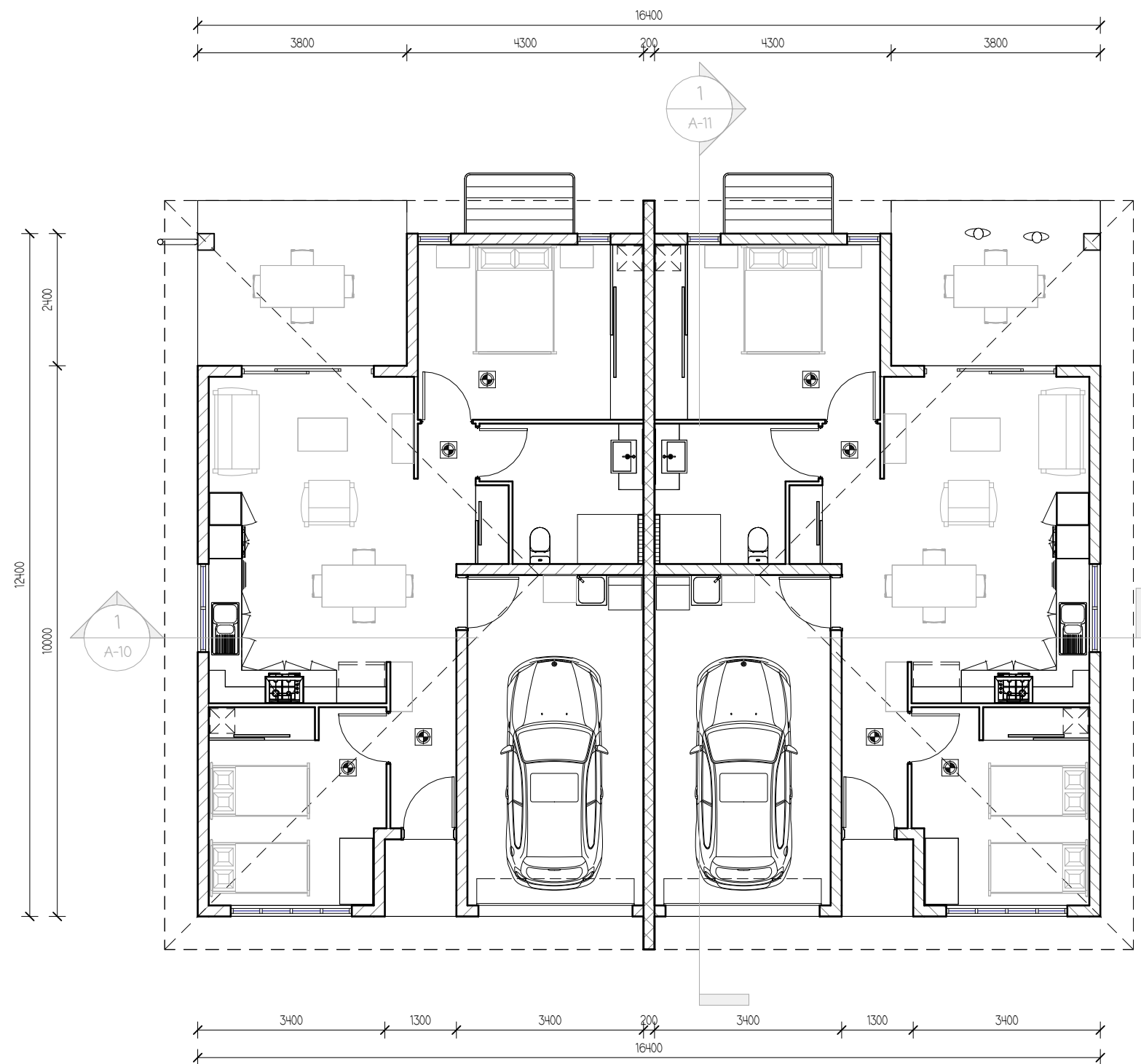
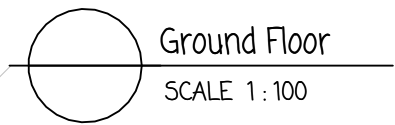
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JOINER TO ENSURE ALL APPLIANCES AND EQUIPMENT FITS IN ALLOCATED SPACINGS, AND TO ADVISE AND ACCOUNT FOR ADJUSTMENTS FOR APPROVED SUBSTITUTIONS

JOINER TO ENSURE ALL APPLIANCES AND EQUIPMENT THAT REQUIRE POWWER, WATER AND/OR WASTE ARE CORRECTLY PROVIDED



ELEVATION KEY

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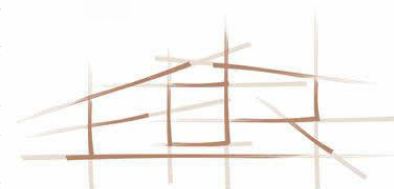
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Floor Area	
<varies>	0 m²
	0 m²

Keynote Legend	
Key Value	Keynote Text

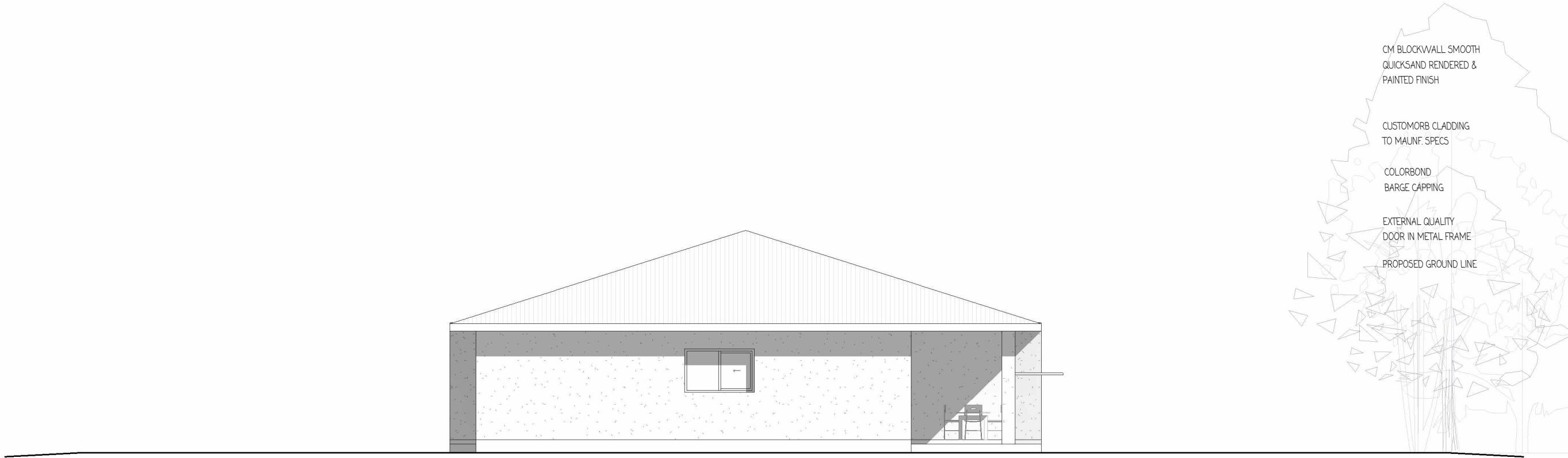
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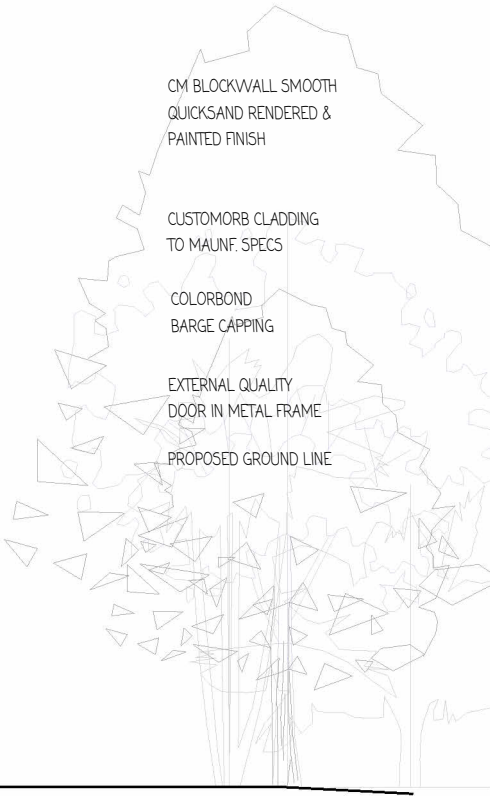
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-Scale:	
-Sheet Number:	A-04 I



Elevation 1



Elevation 2



CM BLOCKWALL SMOOTH
QUICKSAND RENDERED &
PAINTED FINISH

CUSTOMORB CLADDING
TO MAUNF. SPECS

COLORBOND
BARGE CAPPING

EXTERNAL QUALITY
DOOR IN METAL FRAME

PROPOSED GROUND LINE

FEATURE ENTRY DOOR IN
TIMBER FRAME

GABLE VENT.

DOUBLE FEATURE ENTRY
DOOR IN TIMBER FRAME

COLORBOND FASCIA &
GUTTER

GABLE ENDS LINED WITH FC
SHEET & RENDERED. RENDER
JOINT TO BLOCKWORK/SHEET
TRANSITION

FINIAL & FEATURE
GABLE TYPE 'A'

FEATURE ENTRY DOOR IN
METAL FRAME

SELECTED STACKSTONE
FEATURE CLADDING, FIXED TO
MANUF SPECS

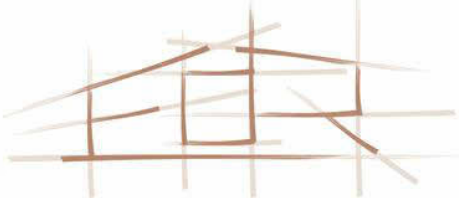
EXISTING/NATURAL
GROUND LINE

SHADOWCLAD
CLADDING TO MAUNF.
SPECS

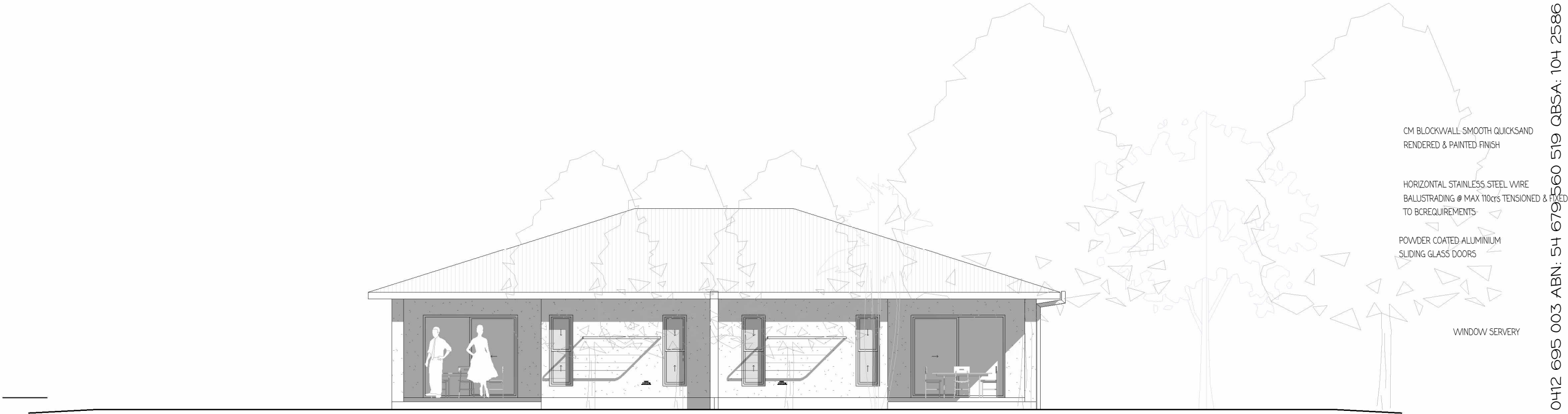
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ISSUES/REVISIONS		

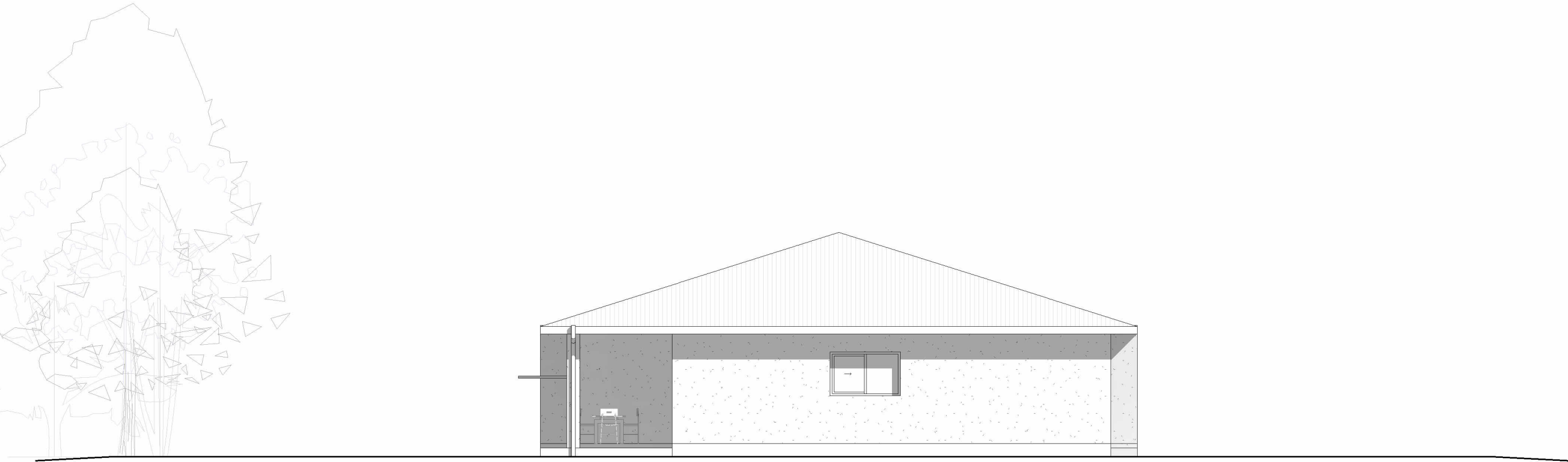
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Elevation 3



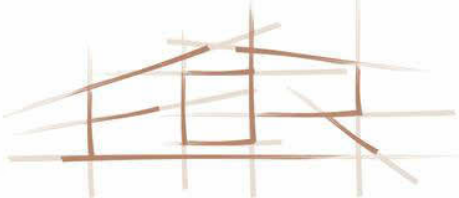
Elevation 4

'TYPE A'

ISSUES/REVISIONS		
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Version: 1, Version Date: 24/08/2026		



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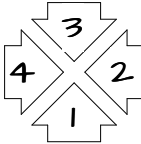


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	ELEVATIONS
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-Project Type:	??
-Client Name:	??
-Project Address:	??

-Project Number:	??
-Drawn By:	Author
-Scale:	AT A3
-Sheet Number:	A-07 I

Key Value	Keynote Text
CL	CLOTHESLINE
CT	COOKTOP
LIN	LINEN
PTY	PANTRY
REF	REFRIDGERATER PROVISIONS ONLY
ROBE	ROBE
SHR	SHOWER
SK	SINK
TUB	LAUNDRY TUB
VB	VANITY BASIN
WC	WATER CLOSET
WM	WASHING MACHINE SPACE



ELEVATION KEY

Floor Area

Living	99.5 m²
Garage/Patio	311 m²
Porch	2.5 m²
	1331 m²

'TYPE B'

GENERAL JOINERY

NOTES & STANDARDS

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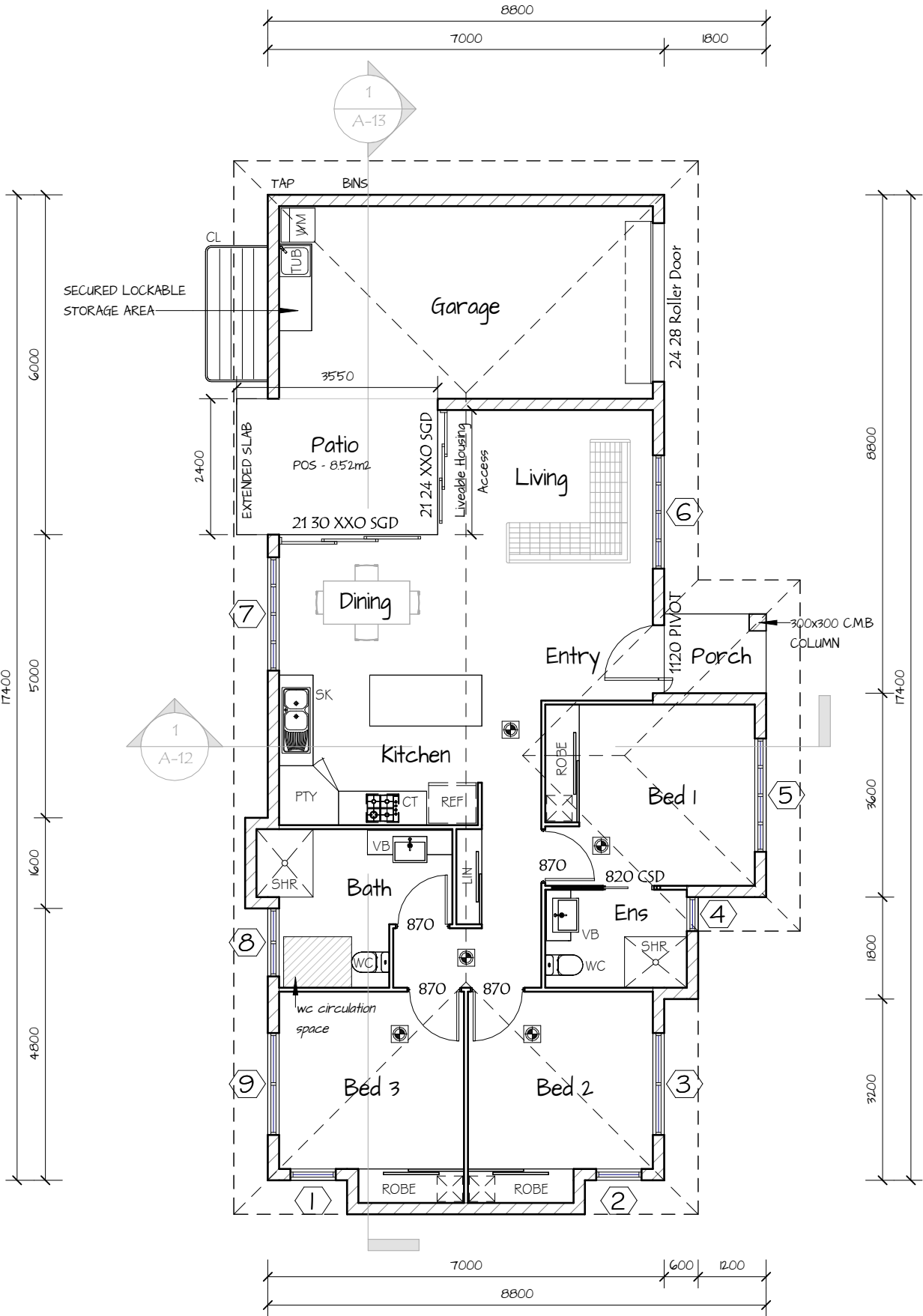
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JOINER TO ENSURE ALL APPLIANCES AND EQUIPMENT FITS IN ALLOCATED SPACINGS, AND TO ADVISE AND ACCOUNT FOR ADJUSTMENTS FOR APPROVED SUBSTITUTIONS

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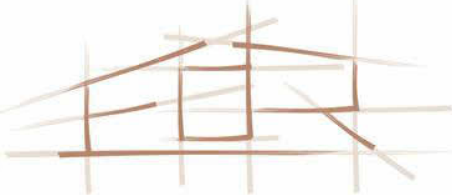
ASSESSMENT ISSUE

Ground Floor
SCALE 1 : 100

ISSUES/REVISIONS



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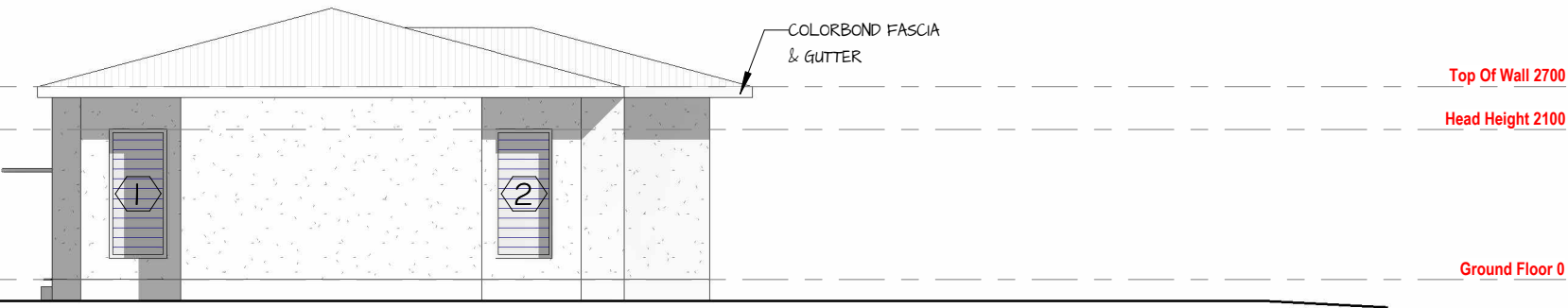


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Elevation 1



Elevation 2



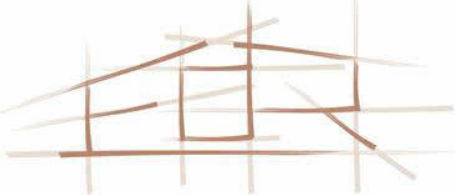
'TYPE B'

ASSESSMENT ISSUE

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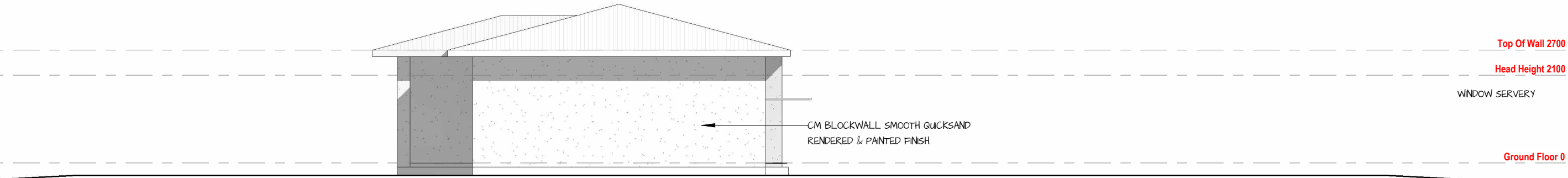
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Elevation 3



Elevation 4



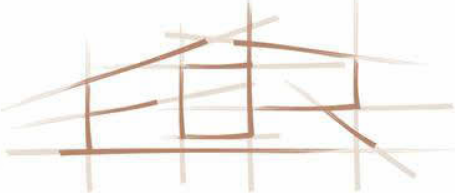
'TYPE B'

ASSESSMENT ISSUE

ISSUES/REVISIONS		



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DESIGN WIND CLASSIFICATION

GENERAL JOINERY NOTES & STANDARDS

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JOINER TO ENSURE ALL APPLIANCES AND EQUIPMENT FITS IN ALLOCATED SPACINGS, AND TO ADVISE AND ACCOUNT FOR ADJUSTMENTS FOR APPROVED SUBSTITUTIONS

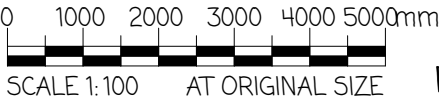
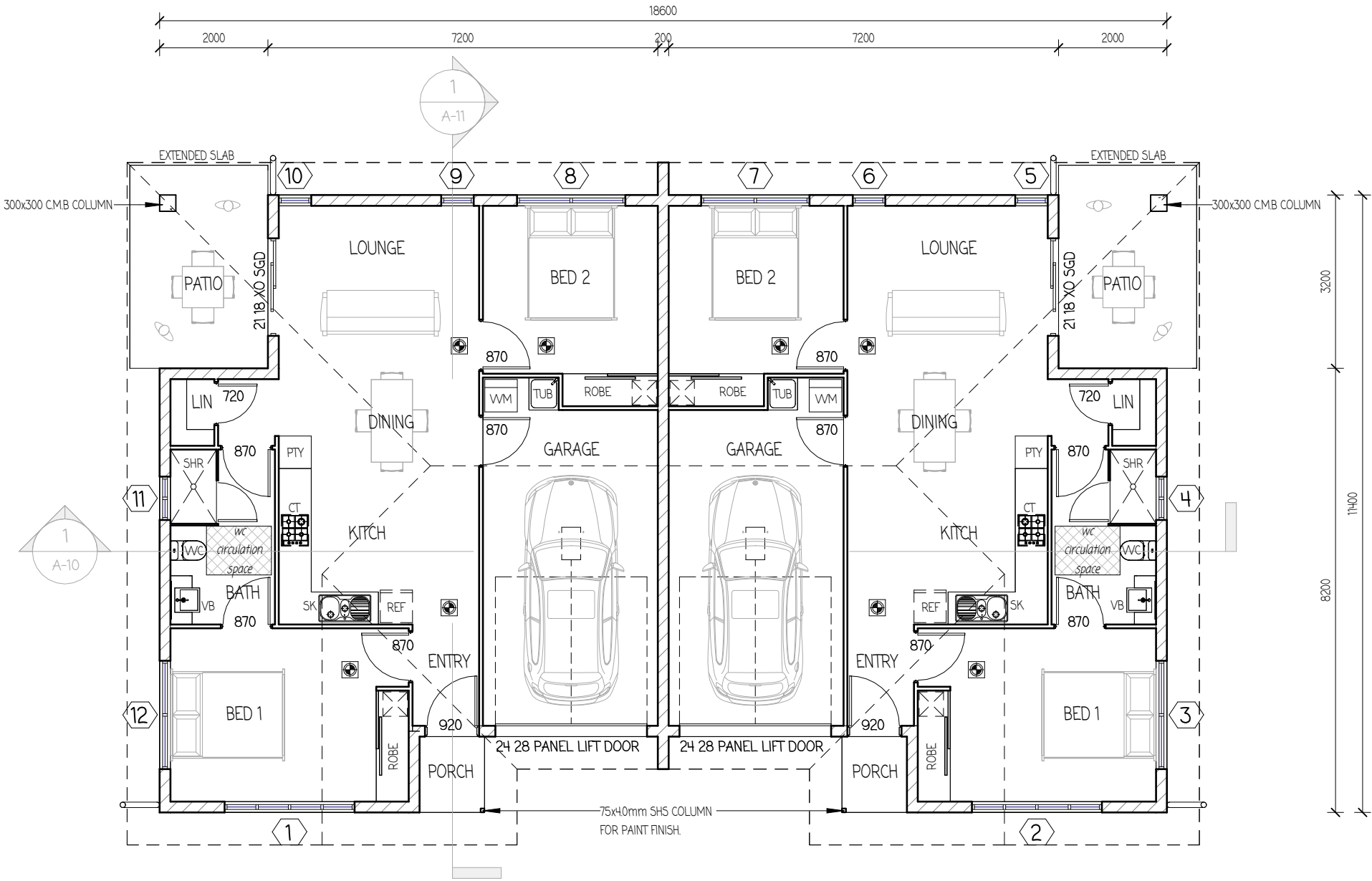
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ASSESSMENT ISSUE

Ground Floor

SCALE 1 : 100

Document Set ID: 4692251
Version: 1, Version Date: 24/08/2026



3

4

2

1

ELEVATION KEY

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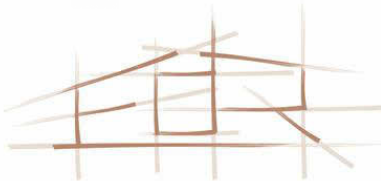
Floor Area	
Living	72.5 m²
Garage	21 m²
Patio	9.6 m²
Porch	1.7 m²
	104.7 m²

Keynote Legend	
Key Value	Keynote Text
CT	COOKTOP
PTY	PANTRY
REF	REFRIDGERATER PROVISIONS ONLY
ROBE	ROBE
SHR	SHOWER
SK	SINK
TUB	LAUNDRY TUB
VB	VANITY BASIN
WC	WATER CLOSET
WM	WASHING MACHINE SPACE

'TYPE C'



Affiliate Level 2
Australian Institute of Architects



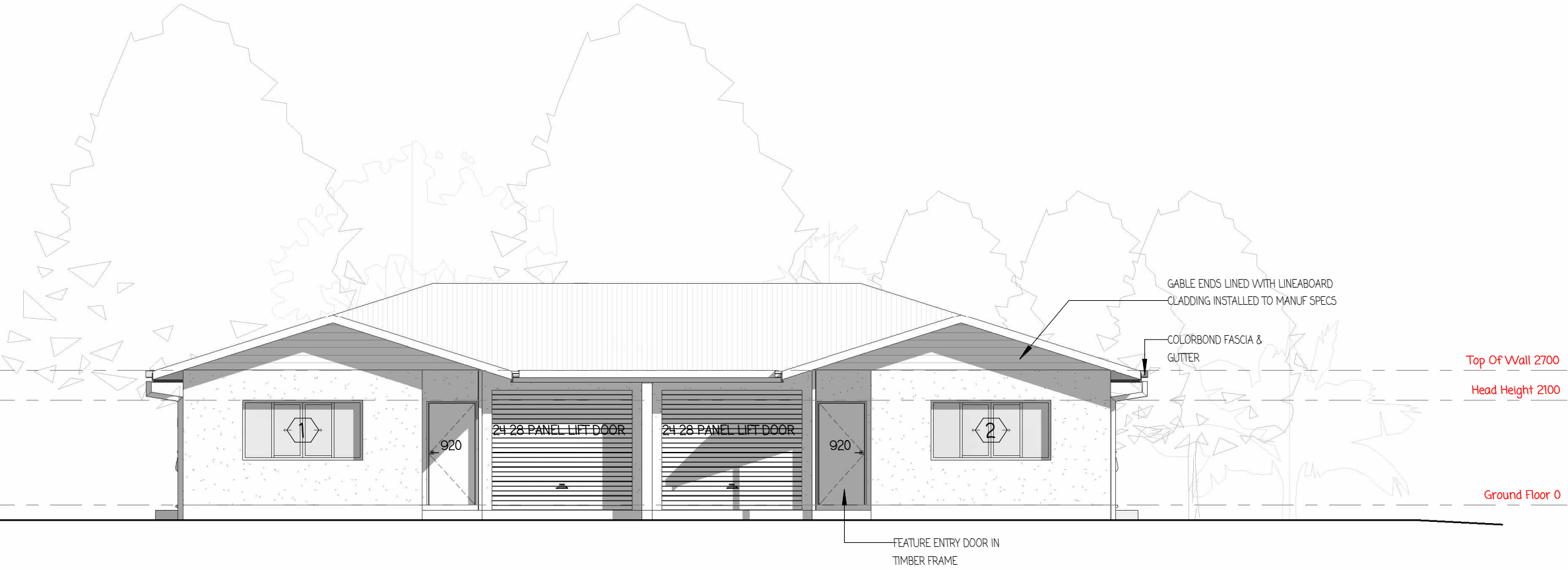
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- Project Type:
- Client Name
- Project Address:
- Drawing Title: FLOOR PLAN
- Scale:
- Sheet Number: A-04 I

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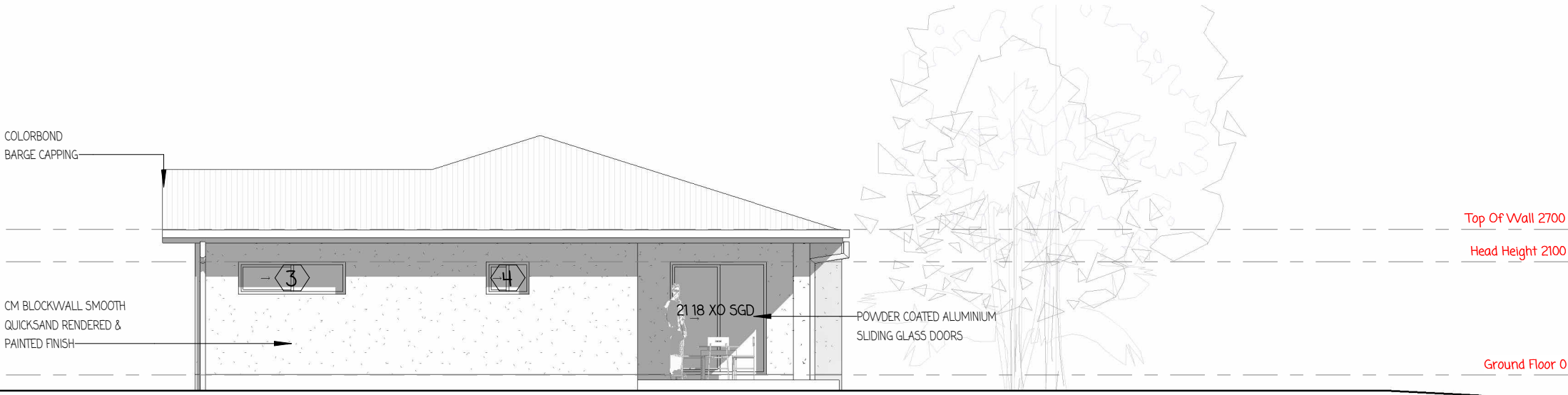
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ASSESSMENT ISSUE

Elevation 1



Elevation 2

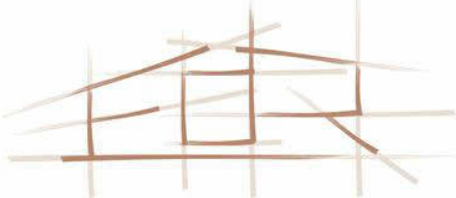


'TYPE C'

ISSUES/REVISIONS		
Document Set ID: 4692251		
Version: 1, Version Date: 24/08/2026		



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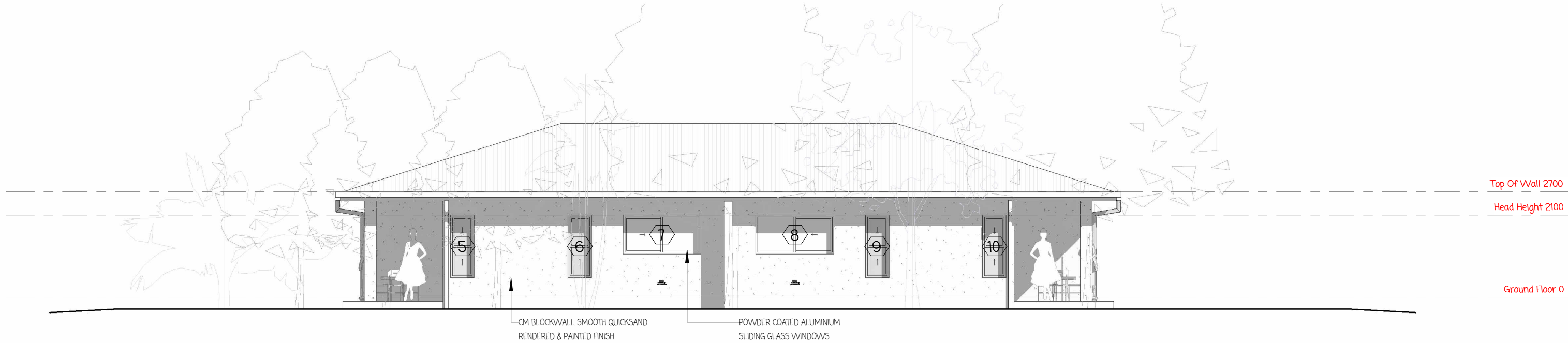


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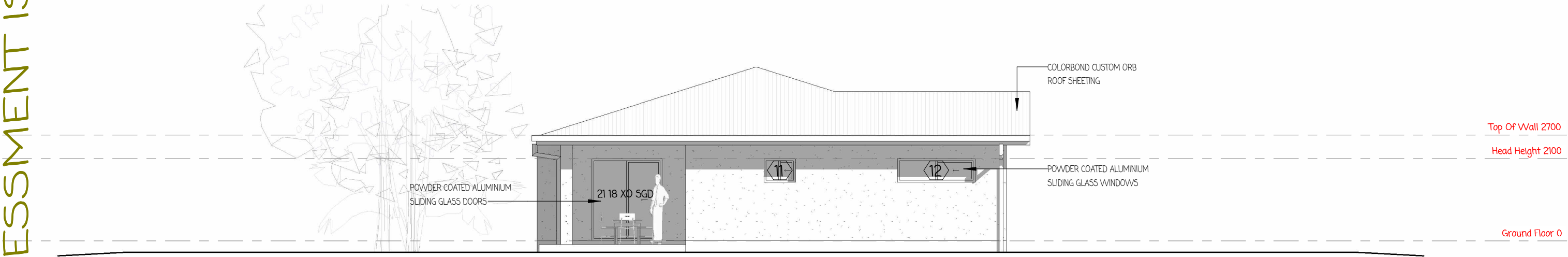
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-Client Name:
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-Project Number:
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-Scale: AT A3
-Sheet Number: A-06 I

ASSESSMENT ISSUE



Elevation 3



Elevation 4

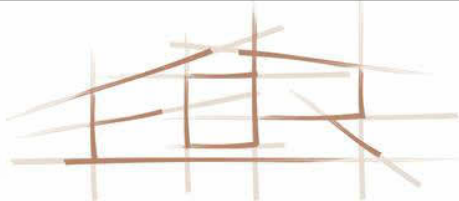
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-Drawing Title: ELEVATIONS
-Project Type:
-Client Name
-Project Address:

-Project Number:
-Drawn By: Author
-Scale: AT A3
-Sheet Number: A-07 I

DESIGN WIND CLASSIFICATION

GENERAL JOINERY NOTES & STANDARDS

CHECK ALL DIMENSIONS AND CONDITIONS ON SITE BEFORE COMMENCEMENT OF ANY BUILDING WORKS AND/OR COMMENCEMENT OF JOINERY SHOP DRAWINGS

CONTRACTOR TO REPORT ANY DISCREPANCIES (ON DRAWING OR ON SITE) BEFORE COMMENCING OF ANY BUILDING WORKS AND/OR COMMENCEMENT OF JOINERY SHOP DRAWINGS

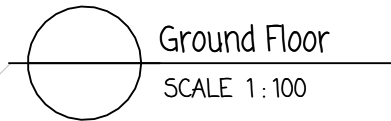
SPECIFIED PROPRIETARY ITEMS DOES NOT IMPLY PREFERENCE FOR THE ITEM INDICATED, BUT IDENTIFIES THE MINIMUM PROPERTIES REQUIRED FOR SUCH ITEMS. ANY SUBSTITUTIONS ARE BY APPROVAL ONLY

CONFIRM NOMINATED APPLIANCES' MANUFACTURER'S RECOMMENDATIONS, SPECIFICATION, REQUIRED SPATIAL REQUIREMENTS AND INSTALLATION REQUIREMENTS WHERE SPECIFIED AS PART OF JOINERY UNIT AND / OR LOCATED ADJACENT TO - REFER JOINERY DRAWINGS AND FLOOR PLAN

JOINER TO ENSURE ALL APPLIANCES AND EQUIPMENT FITS IN ALLOCATED SPACINGS, AND TO ADVISE AND ACCOUNT FOR ADJUSTMENTS FOR APPROVED SUBSTITUTIONS

JOINER TO ENSURE ALL APPLIANCES AND EQUIPMENT THAT REQUIRE POWVER, WATER AND/OR WASTE ARE CORRECTLY PROVIDED

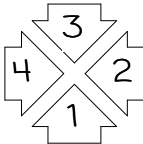
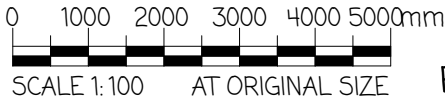
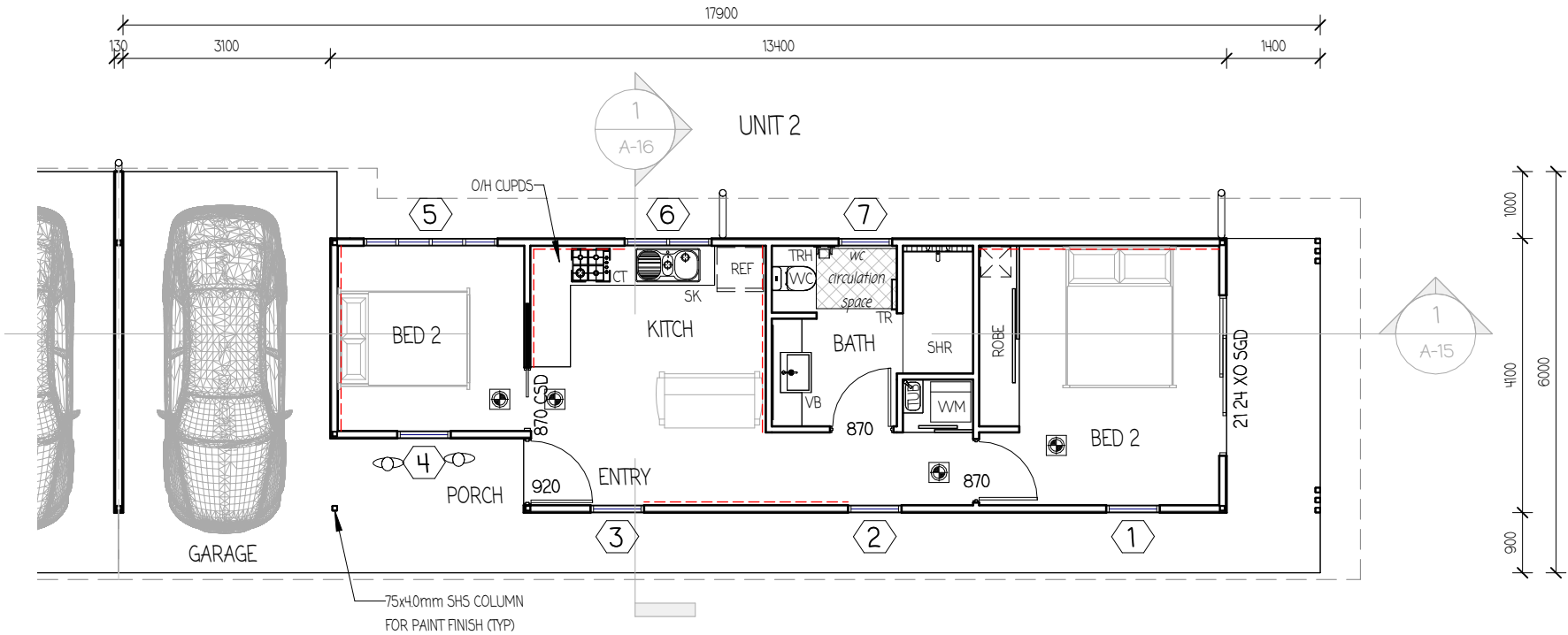
ASSESSMENT ISSUE



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ELEVATION KEY

ISSUES/REVISIONS		

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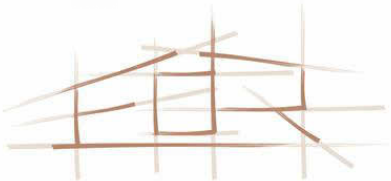
Floor Area	
Ea Unit Living	51.7 m²
Ea Unit Carport, Porch, Walkway	41.4 m²
	93.1 m²

Keynote Legend	
Key Value	Keynote Text
CT	COOKTOP
REF	REFRIDGERATER PROVISIONS ONLY
ROBE	ROBE
SHR	SHOWER
SK	SINK
TR	TOWEL RAIL
TRH	TOILET ROLL HOLDER
TUB	LAUNDRY TUB
VB	VANITY BASIN
WC	WATER CLOSET
WM	WASHING MACHINE SPACE

'TYPE D'



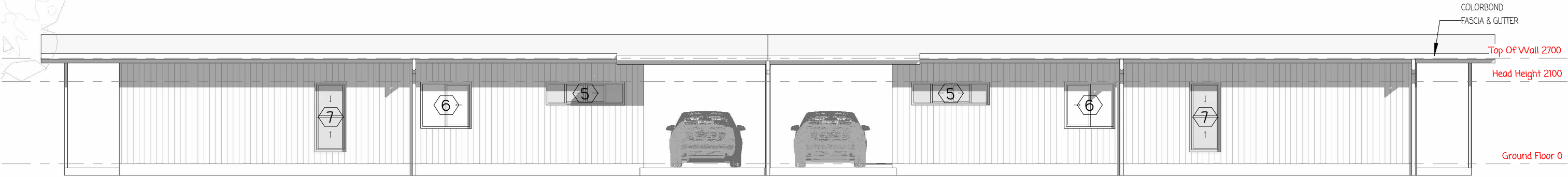
Affiliate Level 2
Australian Institute of Architects



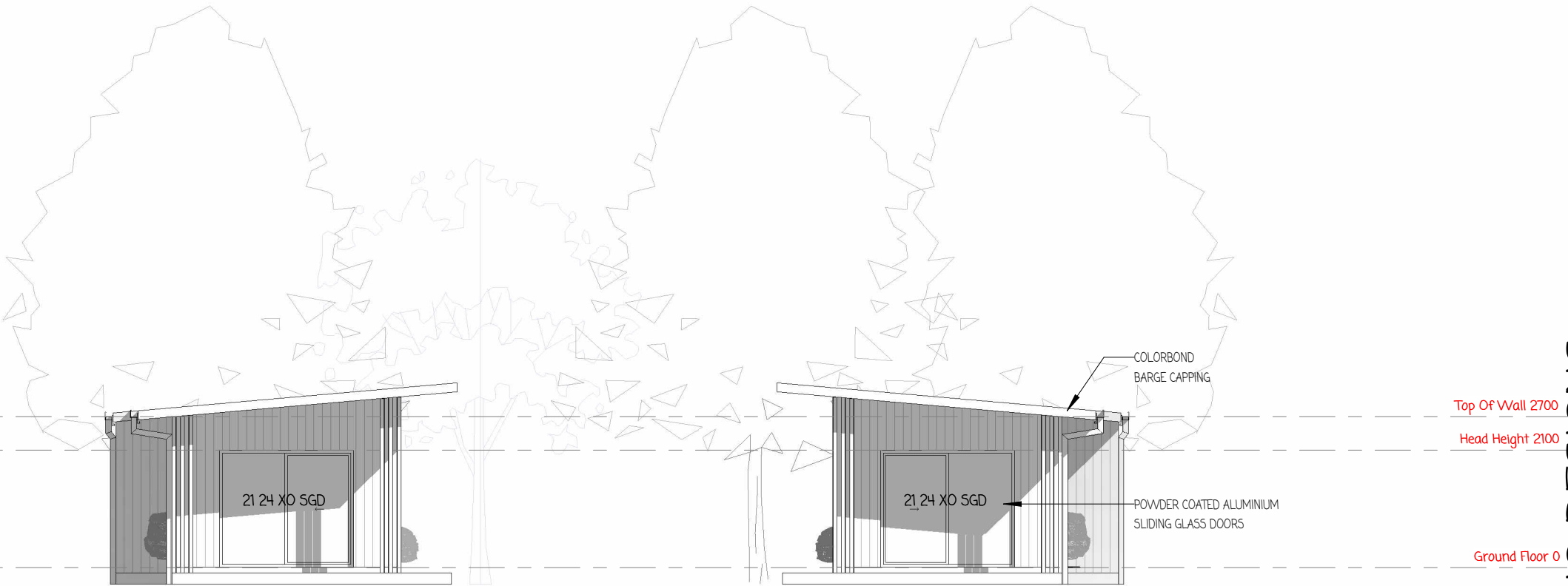
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-Checked By: Designer
-Project Number:
-Project Type:
-Client Name
-Project Address:
-Drawing Title: FLOOR PLAN
-Scale:
-Sheet Number: A-09 I

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Elevation 1



Elevation 2

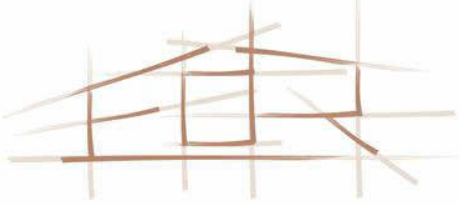
'TYPE D'

ISSUES/REVISIONS		

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		-Client Name:		-Scale:	AT A3
		-Project Address:		-Sheet Number:	A-11

CONSTRUCTION ISSUE

CONSTRUCTION ISSUE



Elevation 3



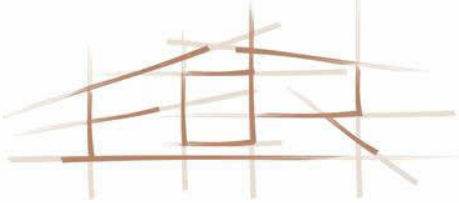
Elevation 4

'TYPE D'

ISSUES/REVISIONS		
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Version: 1, Version Date: 24/08/2026		



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-Drawing Title:

ELEVATIONS

-Project Type:

-Client Name:

-Project Address:

-Project Number:

-Drawn By:

Author

-Scale:

AT A3

-Sheet Number:

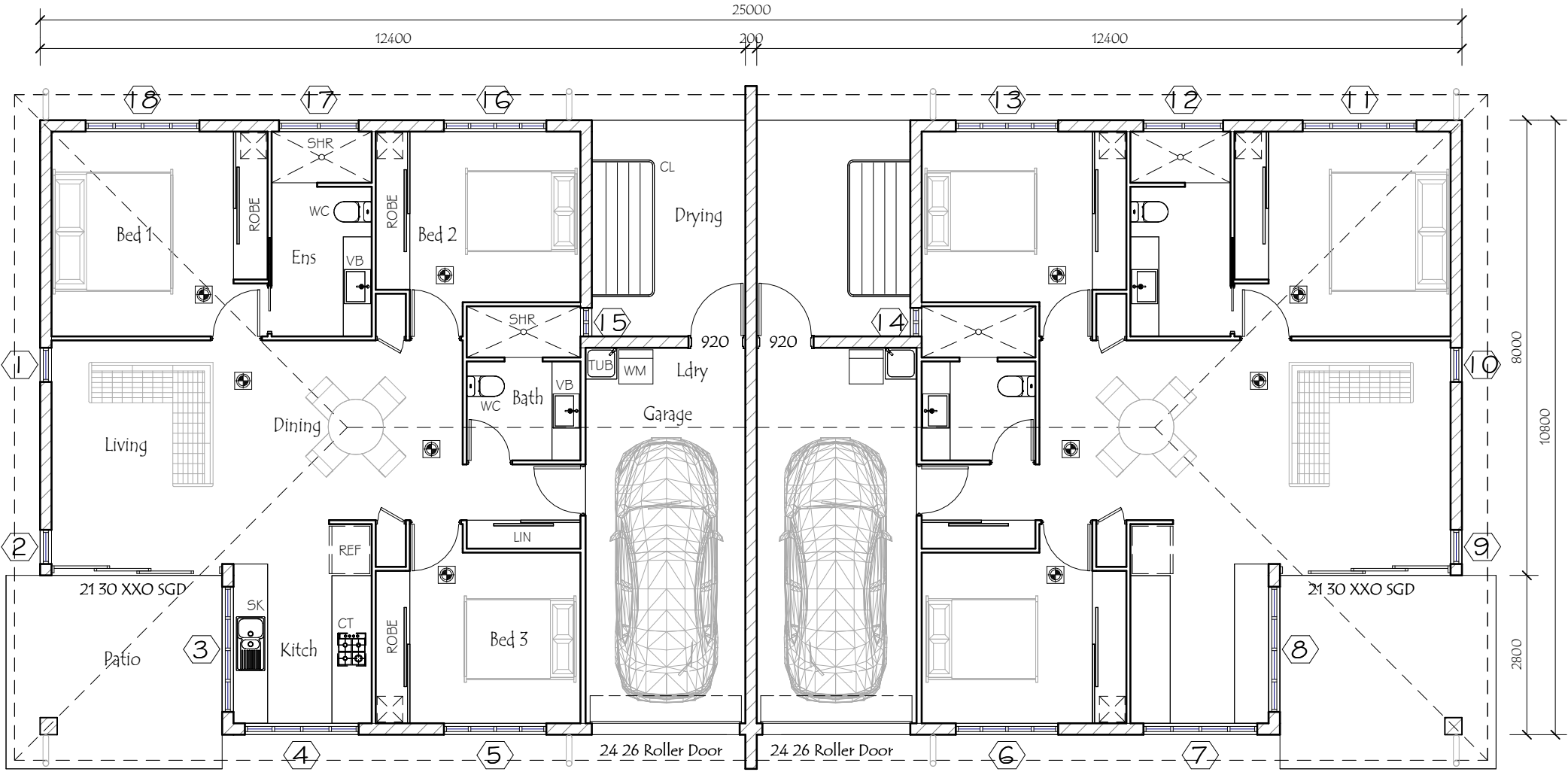
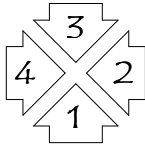
A-12 |

DESIGN WIND CLASSIFICATION

Floor Area

Living	95 m ²
Garage/Drying	31 m ²
Patio	12.9 m ²
	139 m ²

ELEVATION KEY



GENERAL JOINERY
NOTES & STANDARDS

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CONDITIONS ON SITE BEFORE
COMMENCEMENT OF ANY BUILDING
WORKS AND/OR COMMENCEMENT OF
JOINERY SHOP DRAWINGS

CONTRACTOR TO REPORT ANY
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COMMENCEMENT OF JOINERY SHOP
DRAWINGS

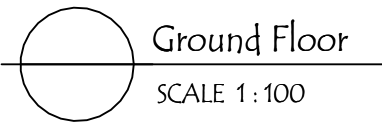
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MINIMUM PROPERTIES REQUIRED FOR
SUCH ITEMS. ANY SUBSTITUTIONS ARE
BY APPROVAL ONLY

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MANUFACTURER'S
RECOMMENDATIONS, SPECIFICATION,
REQUIRED SPATIAL REQUIREMENTS
AND INSTALLATION REQUIREMENTS
WHERE SPECIFIED AS PART OF JOINERY
UNIT AND / OR LOCATED ADJACENT
TO - REFER JOINERY DRAWINGS AND
FLOOR PLAN

JOINER TO ENSURE ALL APPLIANCES
AND EQUIPMENT FITS IN ALLOCATED
SPACINGS, AND TO ADVISE AND
ACCOUNT FOR ADJUSTMENTS FOR
APPROVED SUBSTITUTIONS

JOINER TO ENSURE ALL APPLIANCES
AND EQUIPMENT THAT REQUIRE
POWER, WATER AND/OR WASTE ARE
CORRECTLY PROVISIONED

ASSESSMENT ISSUE



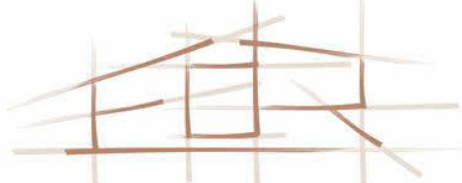
Ground Floor

SCALE 1:100

ISSUES/REVISIONS



CHARTED MEMBER



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-Client Name
-Project Address:

-Project Number:
-Drawing Title: FLOOR PLAN
-Scale: AT A3
-Sheet Number: A-06 I

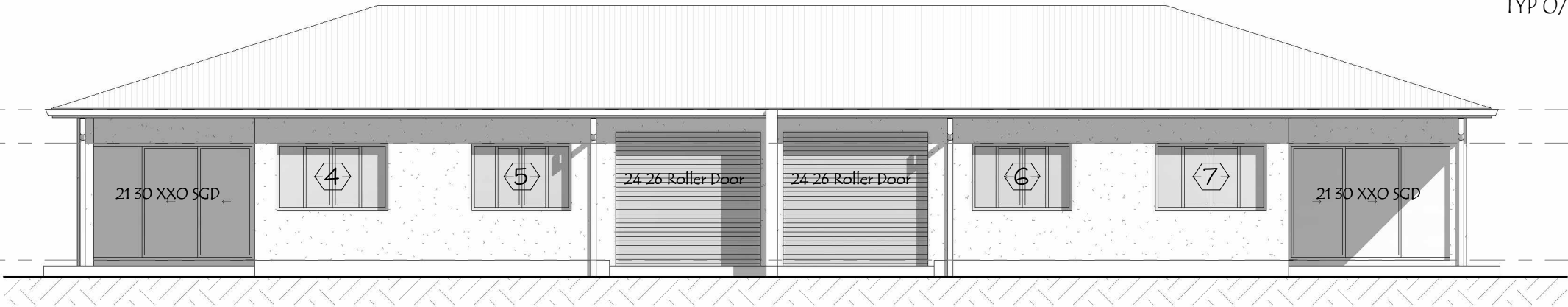
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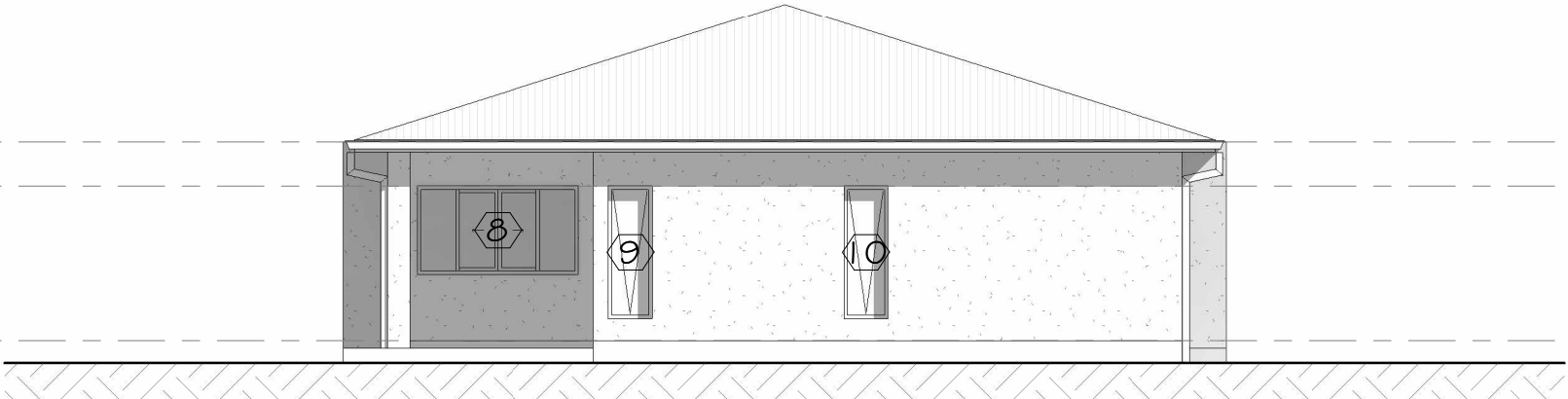
Elevation 1



Top Of Wall 2700
Head Height 2100

Ground Floor

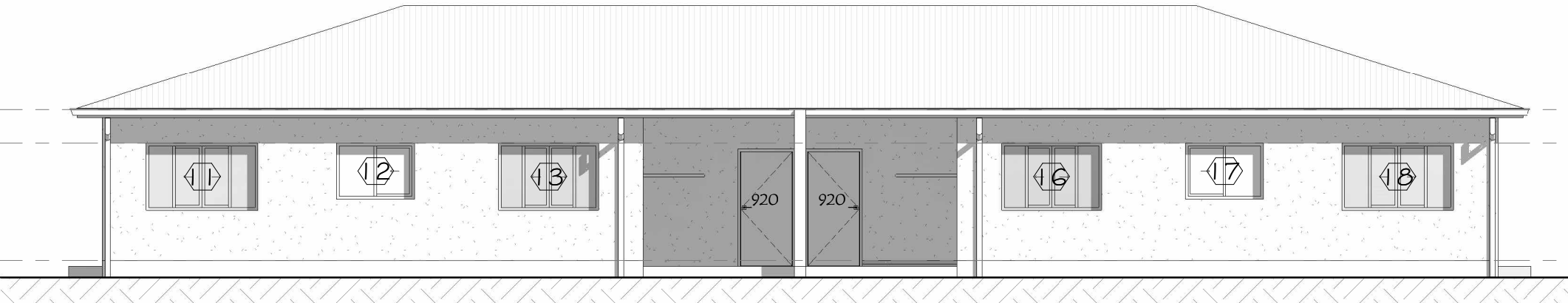
Elevation 2



Top Of Wall 2700
Head Height 2100

Ground Floor

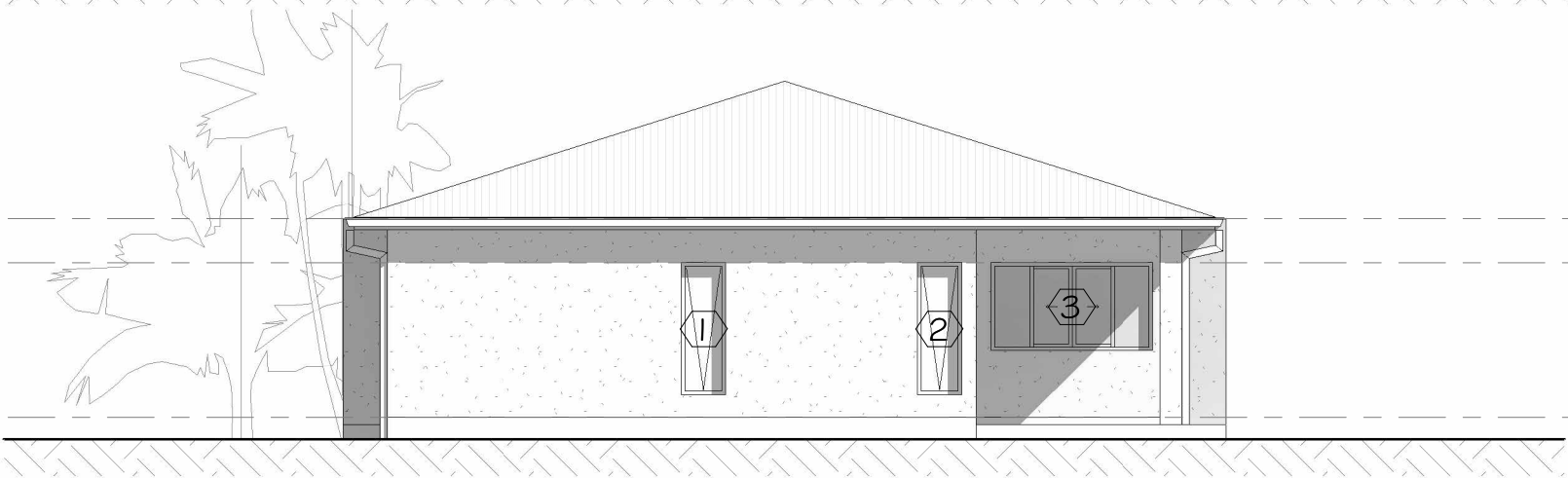
Elevation 3



Top Of Wall 2700
Head Height 2100

Ground Floor

Elevation 4



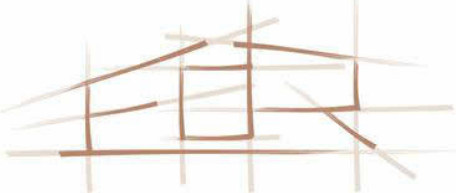
Top Of Wall 2700
Head Height 2100

Ground Floor

'TYPE E'

ASSESSMENT ISSUE

ISSUES/REVISIONS		



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